

## CODE OF CIVIL PROCEDURE SECTION 1245.230.

In addition to other requirements imposed by law, the resolution of necessity shall contain all of the following:

- (a) A general statement of the public use for which the property is to be taken and a reference to the statute that authorizes the public entity to acquire the property by eminent domain.
- (b) A description of the general location and extent of the property to be taken, with sufficient detail for reasonable identification.
- (c) A declaration that the governing body of the public entity has found and determined each of the following:
  - (1) The public interest and necessity require the proposed project.
  - (2) The proposed project is planned or located in the manner that will be most compatible with the greatest public good and the least private injury.
  - (3) The property described in the resolution is necessary for the proposed project.
  - (4) That either the offer required by Section 7267.2 of the Government Code has been made to the owner or owners of record, or the offer has not been made because the owner cannot be located with reasonable diligence.

## Government Code Section 7267.2.

(a) (1) Prior to adopting a resolution of necessity pursuant to Section 1245.230 of the Code of Civil Procedure and initiating negotiations for the acquisition of real property, the public entity shall establish an amount that it believes to be just compensation therefor, and shall make an offer to the owner or owners of record to acquire the property for the full amount so established, unless the owner cannot be located with reasonable diligence. The offer may be conditioned upon the legislative body's ratification of the offer by execution of a contract of acquisition or adoption of a resolution of necessity or both. The amount shall not be less than the public entity's approved appraisal of the fair market value of the property. A decrease or increase in the fair market value of real property to be acquired prior to the date of valuation caused by the public improvement for which the property is acquired, or by the likelihood that the property would be acquired for the improvement, other than that due to physical deterioration within the reasonable control of the owner or occupant, shall be disregarded in determining the compensation for the property.

(2) At the time of making the offer described in paragraph (1), the public entity shall provide the property owner with an informational pamphlet detailing the process of eminent domain and the property owner's rights under the Eminent Domain Law.

(b) The public entity shall provide the owner of real property to be acquired with a written statement of, and summary of the basis for, the amount it established as just compensation. The written statement and summary shall contain detail sufficient to indicate clearly the basis for the offer, including, but not limited to, all of the following information:

(1) The date of valuation, highest and best use, and applicable zoning of property.

(2) The principal transactions, reproduction or replacement cost analysis, or capitalization analysis, supporting the determination of value.

(3) If appropriate, the just compensation for the real property acquired and for damages to remaining real property shall be separately stated and shall include the calculations and narrative explanation supporting the compensation, including any offsetting benefits.

(c) Where the property involved is owner-occupied residential property and contains no more than four residential units, the homeowner shall, upon request, be allowed to review a copy of the appraisal upon which the offer is based. The

(d) Notwithstanding subdivision (a), a public entity may make an offer to the owner or owners of record to acquire real property for less than an amount that it believes to be just compensation therefor if (1) the real property is offered for sale by the owner at a specified price less than the amount the public entity believes to be just compensation therefor, (2) the public entity offers a price that is equal to the specified price for which the property is being offered by the landowner, and (3) no federal funds are involved in the acquisition, construction, or project development.

(e) As used in subdivision (d), "offered for sale" means any of the following

(1) Directly offered by the landowner to the public entity for a specified price in advance of negotiations by the public entity. (2) Offered for sale to the general public at an advertised or published specified price, set no more than six months prior to, and still available at, the time the public entity initiates contact with the landowner regarding the public entity's possible acquisition of the property.



# VISUAL IMPACT ASSESSMENT

## Navarro Ridge Safety Project

November 13, 2018

California Department of Transportation

01, MEN, 1

PM 41.8/42.3

0112000300 EA 01-0C550



Figure 2. Project limits



Figure 5. Adjacent Residence above highway.



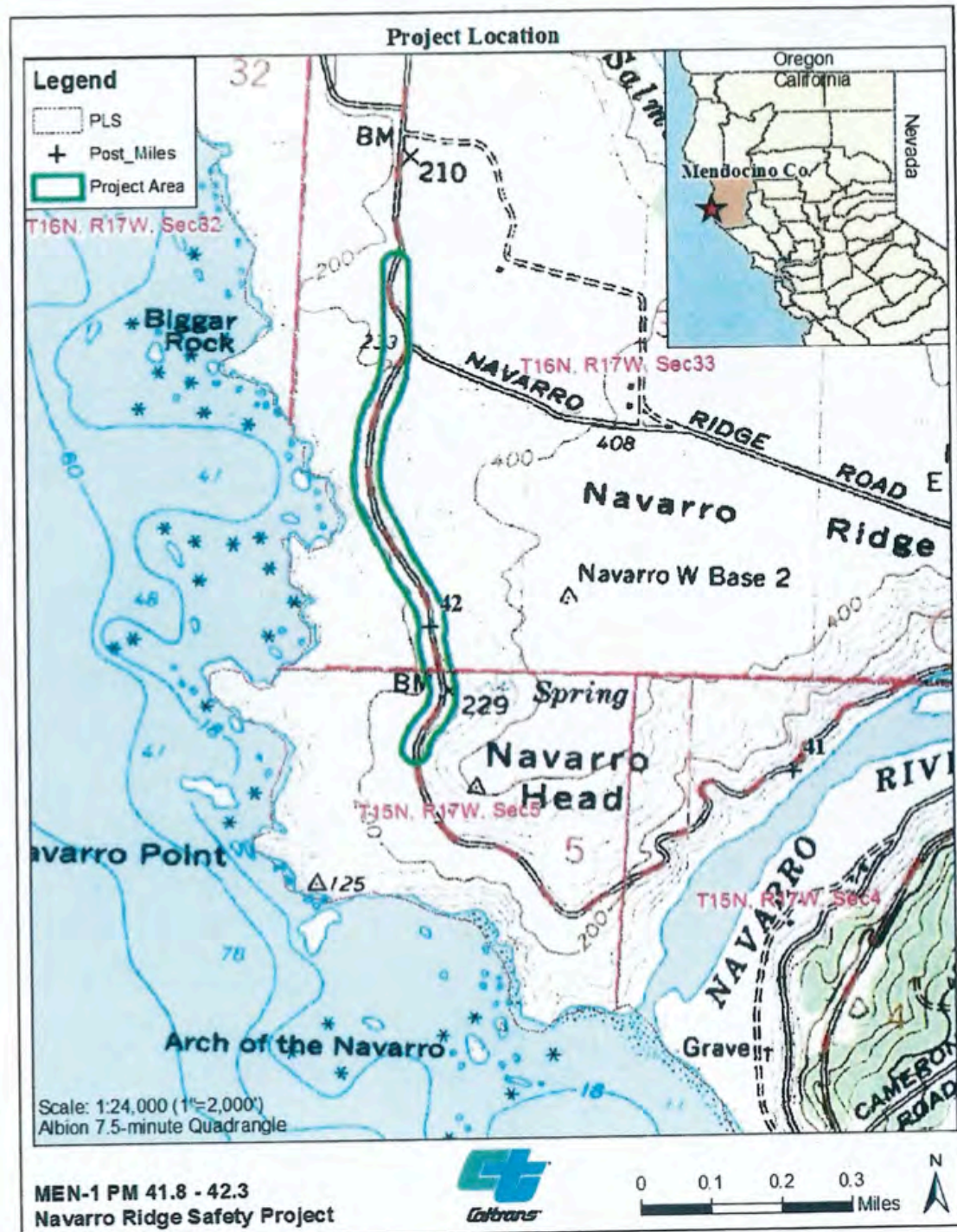


Figure A-1. Project location map.

**Project Location**

**Safety Project**

Reduce frequency and severity of collisions



Franklin A. Engelhardt and Beverly Ann Engelhardt Living Trust  
 (Dall and Associates)

District 1, SR-1  
 Mendocino County

Mike Whiteside  
 Assistant Chief Engineer

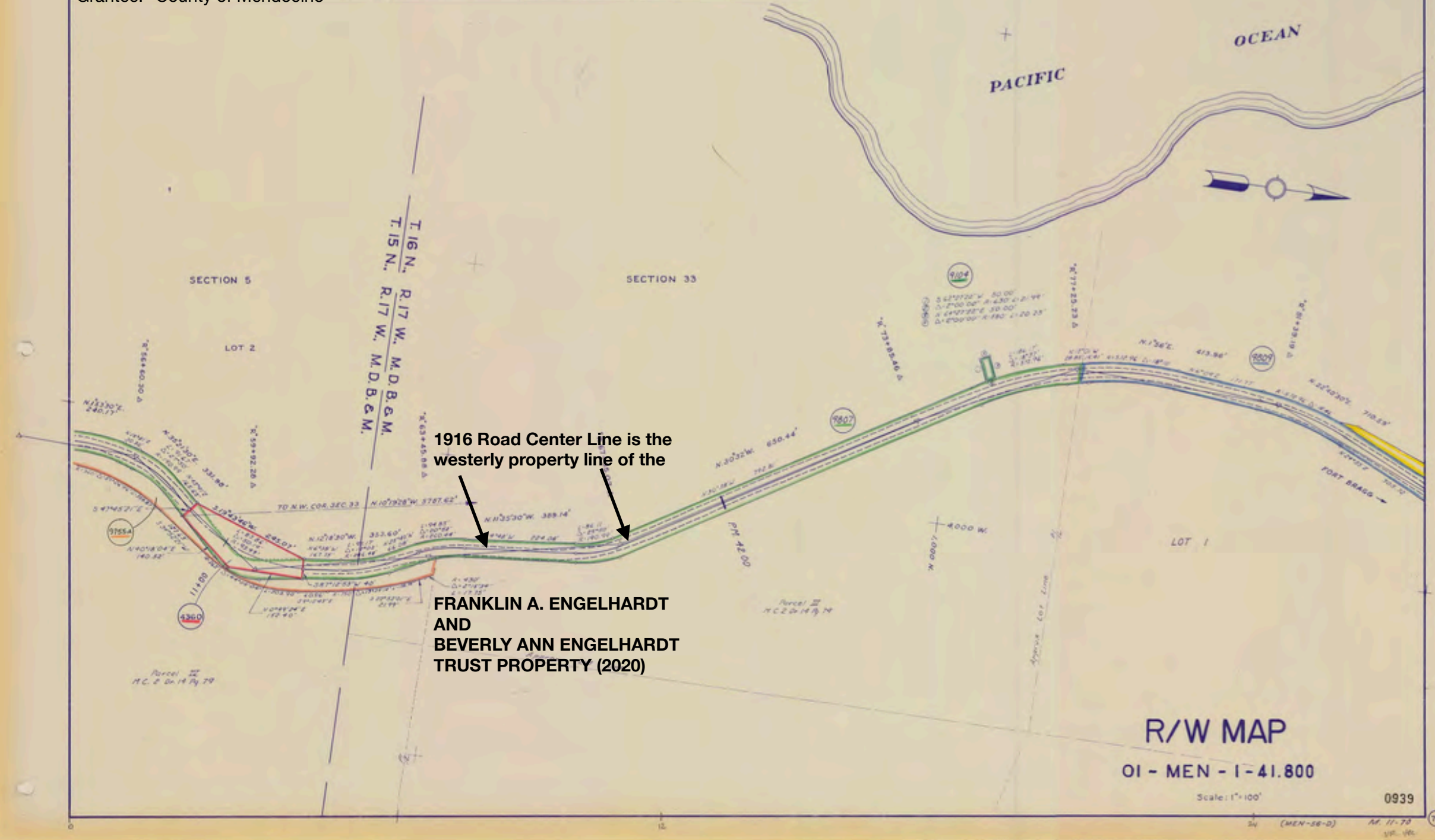


**SCHEDULE**

CASE	GRANTOR	GRANTEE	INSTRUMENT	DATE	RECORD	REMARKS
4360	Pete Jansl	State of California	Grant Deed	3-19-59	Book 509 O.R. Pg. 197	EQC-A
9807	Domenico Rossotti et ux.	County of Mendocino	Grant Deed	4-8-16	160 Deeds Pg. 118	Conveyed Easement Only
9807	Peter Novak et ux.	County of Mendocino	Grant Deed	4-8-16	160 Deeds Pg. 118	Conveyed Easement Only

Grantor: Domenico Rossotti et ux.  
Grantee: "County of Mendocino"

"Conveyed Easement Only"



1916 Road Center Line is the westerly property line of the

FRANKLIN A. ENGELHARDT AND BEVERLY ANN ENGELHARDT TRUST PROPERTY (2020)

EXHIBIT 4. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022

**SCHEDULE**

CASE	GRANTOR	GRANTEE	INSTRUMENT	DATE	RECORD	REMARKS
4360	Pete Sansi	State of California	Grant Deed	3-19-59	Book 509 O.R. Pg. 197	EOC-A
9104	Donald Beaton et al	"	Easement	8-27-87	" 1612 - " 842	CCS 27
9807	Domenico Rossetti et al	County of Mendocino	Grant Deed	4-8-16	" 160 Deeds Pg 118	Conveyed Easement Only
9785A	Zam Enterprises	State of California	Easement	3-22-90	" 185 O.R. Pg 412	E 915-B Sandridge Easement for 75' wide HWY
9809	Peter Nonallo et al	County of Mendocino	Grant Deed	4-8-16	" 160 Deeds Pg 118	Conveyed Easement Only

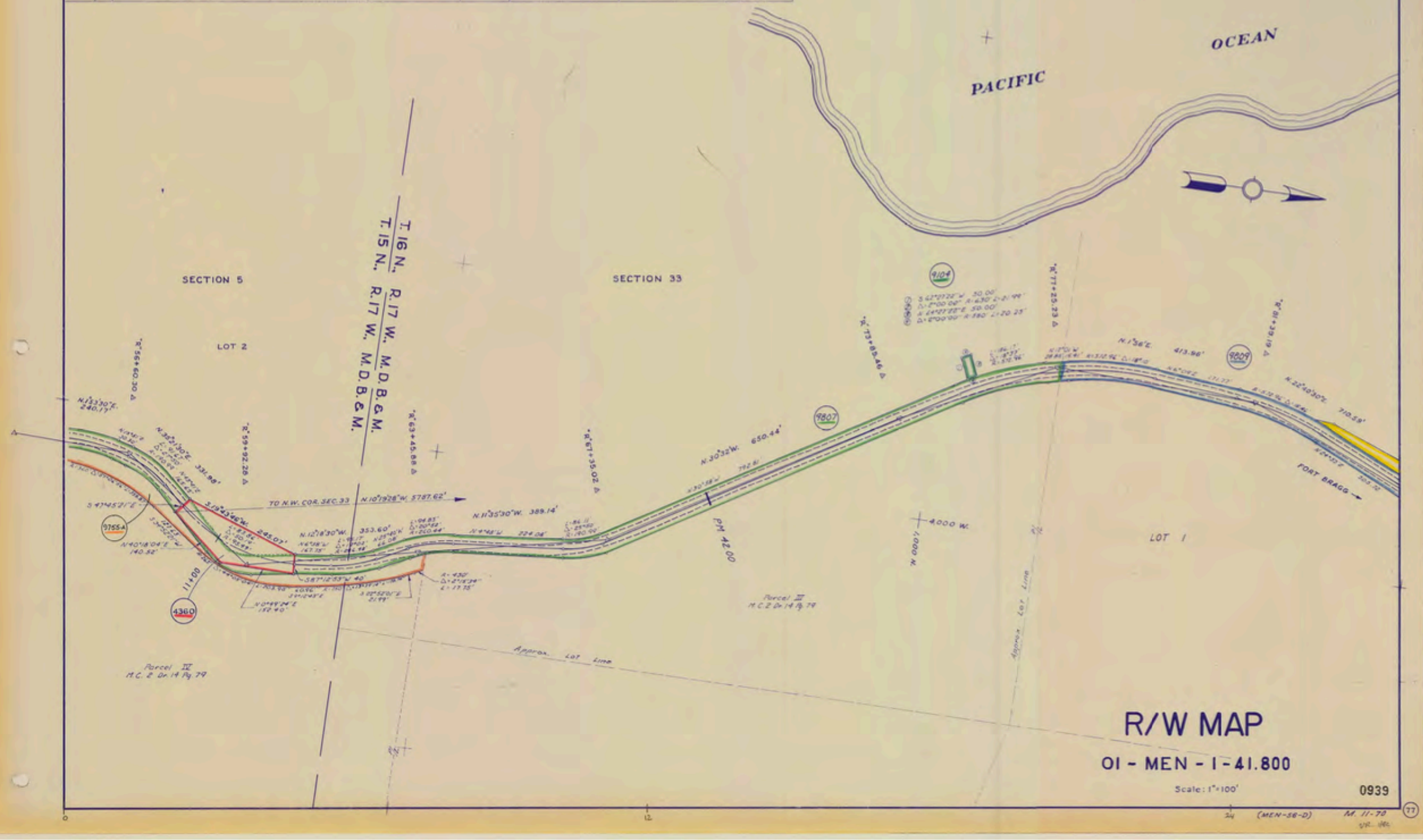
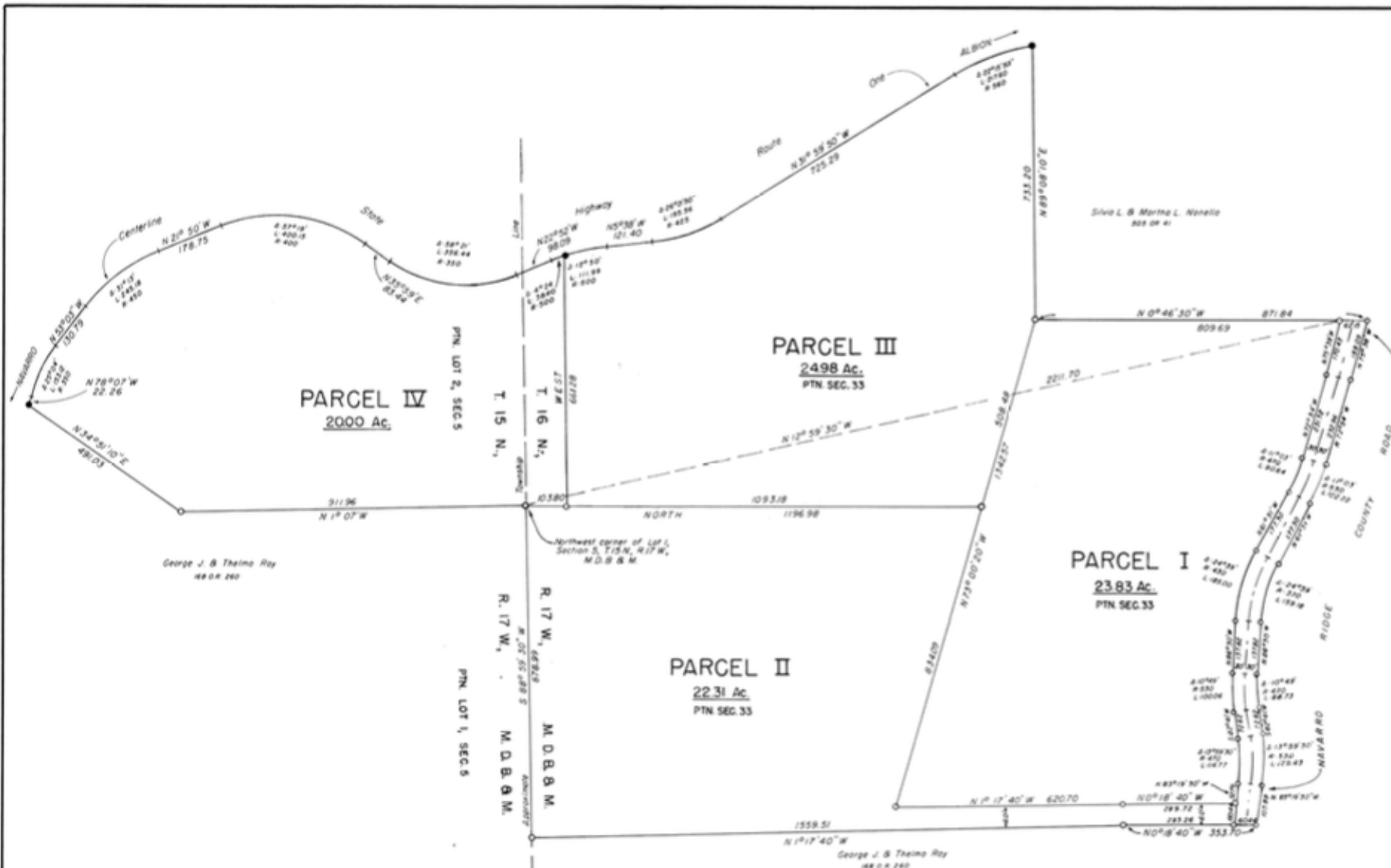


EXHIBIT 5. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022



**BASIS OF BEARING**  
Determined from Solar Observation

**LEGEND**  
 ○ - Set 3/4" Iron Pipe Stamped L.S. 2332  
 ● - Set Bronze Topper Stamped L.S. 2332

**SURVEYOR'S CERTIFICATE**  
 This map was prepared by me or under my direction and is based upon a survey in conformance with the requirements of the Subdivision Map Act at the request of James Sansi. I hereby certify that it conforms to the approved tentative map and the conditions of approval thereof, that all provisions of applicable state laws and local ordinances have been complied with. On December 19, 1969.  
*Oliver O. Howard*  
 Oliver O. Howard  
 L.S. 2332

**COUNTY SURVEYOR'S CERTIFICATE**  
 This map has been examined this 9 day of MARCH, 1970, for conformance with the requirements of section 11575 of the Subdivision Map Act.  
*O.F. Campbell*  
 County Surveyor  
 R.C.E. 16017

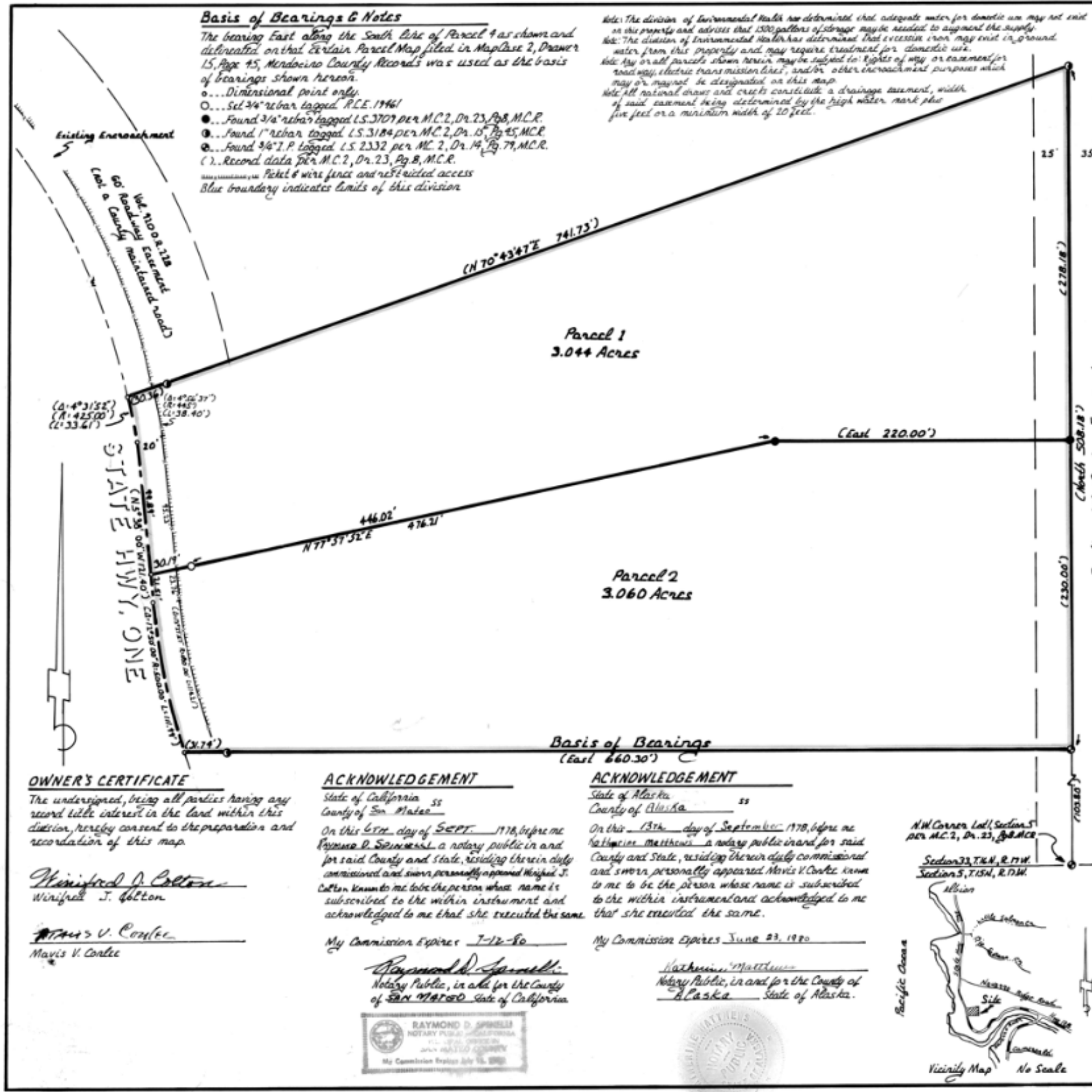
**RECORDER'S CERTIFICATE**  
 Filed at the request of Oliver O. Howard by Dept. File # 1970, on the 9<sup>th</sup> day of March, 1970, at 9:22 o'clock A.M. in Case 2, Drawer 14, Page 79, in the office of the Recorder of the County of Mendocino, State of California.  
*Viola Richardson*  
 County Recorder  
*Dr. W. McLaughlin*  
 Deputy Asst.



COUNTY OF MENDOCINO, STATE OF CALIFORNIA  
 SCALE 1"=200' DATE: February 5, 1970.

CASE	2
DRAWER	14
PAGE	79





**Basis of Bearings & Notes**  
 The bearing East along the South Line of Parcel 4 as shown and delineated on that certain Parcel Map filed in Map Case 2, Drawer 15, Page 45, Mendocino County Records was used as the basis of bearings shown hereon.  
 o... Dimensional point only.  
 o... Set 3/4" rebar tagged P.C.E. 19461  
 o... Found 3/4" rebar tagged L.S. 3707 per M.C. 2, Dr. 23, Pg. 8, M.C.R.  
 o... Found 1" rebar tagged L.S. 3184 per M.C. 2, Dr. 15, Pg. 45, M.C.R.  
 o... Found 3/4" P. tagged L.S. 2332 per M.C. 2, Dr. 14, Pg. 79, M.C.R.  
 ( )... Record data per M.C. 2, Dr. 23, Pg. 8, M.C.R.  
 --- Picket & wire fence and restri. access  
 Blue boundary indicates limits of this division

Note: The division of Environmental Health has determined that adequate water for domestic use may not exist on this property and advises that 1500 gallons of storage may be needed to augment the supply.  
 Note: The division of Environmental Health has determined that excessive iron may exist in ground water from this property and may require treatment for domestic use.  
 Note: Any or all parcels shown herein may be subject to rights of way or easement for roadway, electric transmission lines, and/or other encroachment purposes which may or may not be designated on this map.  
 Note: All natural draws and creeks constitute a drainage easement, width of said easement being determined by the high water mark plus five feet or a minimum width of 20 feet.

**SURVEYOR'S CERTIFICATE**  
 This map was prepared by me or under my direction and is based upon a field survey in conformance with the requirements of the Subdivision Map Act at the request of Winifred Colton and Mavis V. Conlee in April of 1978. I hereby state that the parcel map procedures of local agency have been complied with and that this Parcel Map conforms to the approved tentative map and the conditions of approval thereof which were required to be fulfilled prior to the filing of this parcel map.  
 Ivan Lee Welby, Jr., R.C.E. 19461

**ACCURACY CERTIFICATE**  
 I, Ivan Lee Welby, Jr., hereby certify that all survey work required in the preparation of this map and related monumentation was performed to an accuracy of one in five thousand.  
 Ivan Lee Welby, Jr., R.C.E. 19461

**COUNTY SURVEYOR'S CERTIFICATE**  
 This map conforms with the requirement of the Subdivision Map Act and local ordinances.  
 C.F. Campbell, County Surveyor  
 County of Mendocino, State of California  
 Dated September 22, 1978  
 by Lawrence B. Parsby, Deputy  
 L.S. 3234

**DIRECTOR OF PLANNING'S CERTIFICATE**  
 This map substantially conforms to the approved tentative map and the conditions of approval thereof.  
 Gary De Salvatore, Director of Planning and Building  
 Inspection, Mendocino County, State of California  
 Dated Sept. 22, 1978  
 by Richard G. Hilde, Deputy

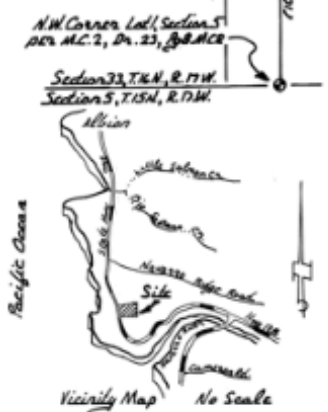
**COUNTY RECORDER'S CERTIFICATE**  
 Filed this 4 day of October 1978, at 3:18 P.M. in Map Case 2, Drawer 33, Page 43, at the request of Winifred Colton and Mavis V. Conlee.  
 Viola Richardson,  
 County Recorder  
 by J. Bennett, Deputy

**PROOF OF OWNERSHIP CERTIFICATE**  
 I hereby certify that the proof of ownership report made by Winifred Colton and Mavis V. Conlee on this 4 day of October, 1978, conforms to the provisions of the Subdivision Map Act.  
 signed Viola Richardson  
 County Recorder  
 by J. Bennett, Deputy

**PARCEL MAP**

Of M.S. 9778 Reverting to acreage and resubdivision of Parcels 1, 2, 3, and 4 of Parcel Map filed in Map Case 2, Drawer 23, Page 8, Mendocino County Record and being a division of Parcel 4 as delineated on that certain Parcel Map filed in Map Case 2, Drawer 15, Page 45, Mendocino County Records, being a portion of Section 33, Township 16, North, Range 17, West, Mount Diablo Base and Meridian.

Mendocino County California  
 Scale - 1" = 50' April 1978  
 Sheet one of one



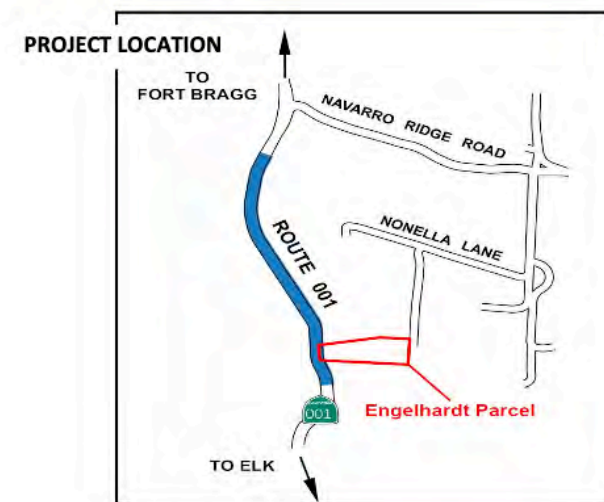
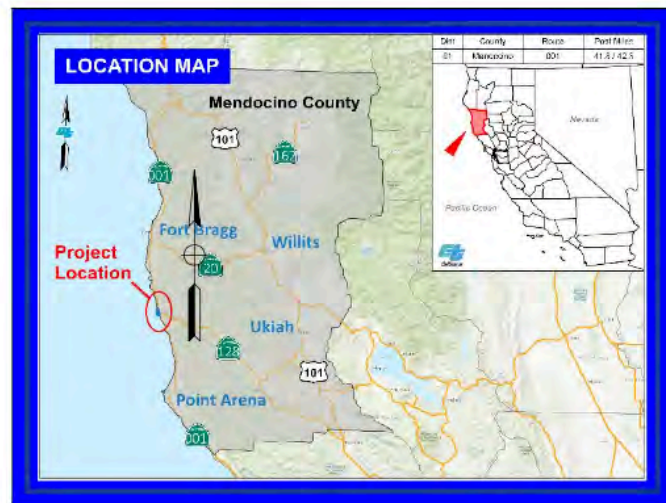
**OWNER'S CERTIFICATE**  
 The undersigned, being all parties having any record title interest in the land within this division, hereby consent to the preparation and recordation of this map.  
 Winifred J. Colton  
 Mavis V. Conlee

**ACKNOWLEDGEMENT**  
 State of California ss  
 County of San Mateo  
 On this 6th day of SEPT. 1978, before me Raymond D. Spirelli, a notary public in and for said County and State, residing therein duly commissioned and sworn personally appeared Winifred J. Colton known to me to be the person whose name is subscribed to the within instrument and acknowledged to me that she executed the same.  
 My Commission Expires 7-12-80

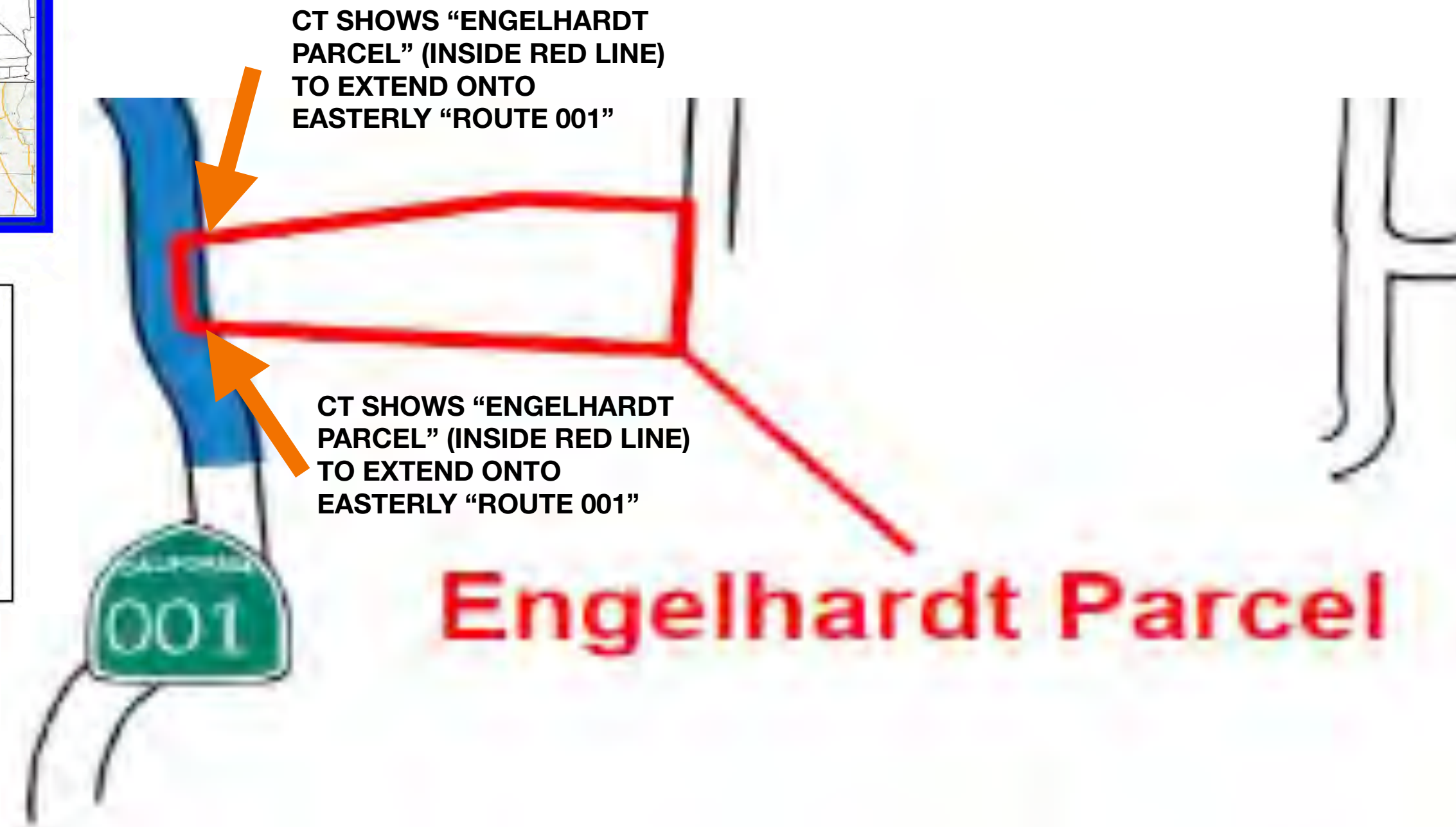
**ACKNOWLEDGEMENT**  
 State of Alaska ss  
 County of Alaska  
 On this 13th day of September, 1978, before me Nathaniel Matthews, a notary public in and for said County and State, residing therein duly commissioned and sworn personally appeared Mavis V. Conlee known to me to be the person whose name is subscribed to the within instrument and acknowledged to me that she executed the same.  
 My Commission Expires June 23, 1980



Map Case 2  
 Drawer 33  
 Page 43  
 Fee 5.00  
 21477



**EXHIBIT A**



Source: Excerpt (lower frame) from Caltrans PPP to CTC, 20201022, Exhibit A: "Engelhardt Parcel" extends onto Caltrans-shown "Route 001"



**CATEGORICAL EXEMPTION/CATEGORICAL EXCLUSION DETERMINATION FORM**

01-MEN-1 District/County/Route:	41.8/42.3 PM/PM:	01-0C550	N/A Federal-Aid Project Number
<b>PROJECT DESCRIPTION:</b> <i>(Briefly describe project including need, purpose, location, limits, and right-of-way requirements.)</i>			
<p>The proposed project would make safety improvements and perform drainage rehabilitation on State Route 1 (SR 1) between post miles (PM) 41.8 and 42.3 in Mendocino County. The project is being proposed to reduce the number and severity of collisions by improving roadway geometry, increasing shoulder widths, and installing rumble strips and a Midwest Guardrail System (MGS). Work would include widening both lanes to 12 feet, widening both road shoulders to 4 feet, and improving the super elevation of the road surface. The construction of the new shoulders will require excavation of material from the east side of the road and placement of a new structural section.</p> <p align="center">See next page for additional project information.</p>			

July 1, 2019/Notice of Decision to Inspect:

Dear Property Owners:

The California Department of Transportation (Department) is proposing a safety improvement project which includes lane and shoulder widening.

December 10, 2019/Offer Cover letter:

Dear Mrs. Engelhardt:

The California Department of Transportation (Caltrans) is proposing a safety project along Hwy 1 near Albion and Navarro Ridge Road which includes lane and shoulder widening. The Franklin & Beverly Engelhardt Trust property located at 33711 Navarro Ridge Road is within the project area and portions are required for the project in Fee (0.15 acre). This area is identified in the enclosed Right of Way Appraisal Map as Parcel 12967-1 and is shaded in brown.

Appraisal/Cost Reimbursement Agreement:

**STATE plans a safety improvement project that includes lane and shoulder widening. The PARCEL is required for the project. STATE has the authority to exercise the power of eminent domain and has made an offer to purchase the PARCEL. Pursuant to C.C.P. 1263.025 OWNER(S)/APPRAISER is entitled to be paid/reimbursed up to**

August 14, 2019/Appraisal Report Page 5:

**PROJECT DESCRIPTION:** This project proposes to improve the safety of State Route (SR) 1 from 1.5 miles north of the junction of SR 128 and SR 1 to 0.1 mile south of Navarro Ridge Road, in Mendocino County, near Albion. The project improvements include the following: widen the shoulders to 4 feet, replace or modify drainage inlets and culverts, install edge line rumble strips, replace the existing center-line rumble strip, improve super-elevation, remove trees, and install a Midwest Guardrail System.

Summary Statement:

STATE OF CALIFORNIA • DEPARTMENT OF TRANSPORTATION

**SUMMARY STATEMENT RELATING TO THE PURCHASE OF REAL PROPERTY OR AN INTEREST THEREIN**

(Form #)

EXHIBIT

8-EX-16 (REV 3/2017)

Page 1 of 2

The California Department of Transportation is proposing a safety improvement project which includes land and shoulder widening.

Your property located at 33711 Navarro Ridge Road, Albion CA is within the project area, and is also identified by your county assessor as Parcel No. 123-310-16.

Grant Deed (Page 2):

The GRANTOR further understands that the present intention of the STATE is to construct and maintain a public highway on the lands hereby conveyed in fee and the GRANTOR, for itself and its successors and assigns, hereby waives any claim for any and all damages to GRANTOR'S remaining property contiguous to the property hereby conveyed by reason of the location, construction, landscaping or maintenance of the highway.

July 1, 2019

Dist.-Co.-Rte.:	<u>01-MEN-1</u>
E.A./P.I.D.:	<u>0C550 / 0112000300</u>
Parcel No.:	<u>12967</u>

Frank & Beverly Engelhardt  
P.O. Box 851  
Albion, CA 95410-0851

Dear Property Owners:

The California Department of Transportation (Department) is proposing a safety improvement project which includes lane and shoulder widening.

Your property, located at 33711 Navarro Ridge Rd., Albion, is within the project area, and a portion is required for the project.

STATE OF CALIFORNIA • DEPARTMENT OF TRANSPORTATION  
**SUMMARY STATEMENT RELATING TO THE PURCHASE OF  
REAL PROPERTY OR AN INTEREST THEREIN**  
(Form #)

EXHIBIT  
8-EX-16 (REV 3/2017)  
Page 1 of 2

The California Department of Transportation is proposing a safety improvement project which includes land and shoulder widening.

Your property located at 33711 Navarro Ridge Road, Albion CA is within the project area, and is also identified by your county assessor as Parcel No. 123-310-16.

Dear Property Owners:

The California Department of Transportation (Department) is proposing a safety improvement project which includes lane and shoulder widening.

Dear Mrs. Engelhardt:

The California Department of Transportation (Caltrans) is proposing a safety project along Hwy 1 near Albion and Navarro Ridge Road which includes lane and shoulder widening. The Franklin & Beverly Engelhardt Trust property located at 33711 Navarro Ridge Road is within the project area and portions are required for the project in Fee (0.15 acre). This area is identified in the enclosed Right of Way Appraisal Map as Parcel 12967-1 and is shaded in brown.

**PROJECT DESCRIPTION:** This project proposes to improve the safety of State Route (SR) 1 from 1.5 miles north of the junction of SR 128 and SR 1 to 0.1 mile south of Navarro Ridge Road, in Mendocino County, near Albion. The project improvements include the following: widen the shoulders to 4 feet, replace or modify drainage inlets and culverts, install edge line rumble strips, replace the existing center-line rumble strip, improve super-elevation, remove trees, and install a Midwest Guardrail System.

**PROPERTY DESCRIPTION:** The subject property consists of approximately 3.06 acres zoned Rural Residential (5) and is currently being used as a single-family residence. The subject is located on the east side of Highway 1 with access off of Navarro Ridge Road. The topography is sloping with some native trees, shrubs, and grass. There are no improvements within the acquisition area.



Navarro Ridge Safety Project  
**Project Study Report Cost Estimate**



01-MEN-1  
 PM 41.77/42.33  
 EA: 01-0C550K  
 EFIS: 01 1200 0300  
 Program Code: 20.XX.201.010  
 August 17, 2015

**PROJECT DESCRIPTION:** Navarro MBGR and Widening



**LIMITS:** In Mendocino County near Albion on MEN 1 from PM 41.77 to 42.33, approximately 1.5 miles north of the junction of SR 128 and SR 1 to 0.1 miles south of Navarro Ridge Road.

**PROPOSED IMPROVEMENT (SCOPE):** The scope of work proposes to widen existing shoulders in both directions to 4', install edgeline rumblestrip, and install metal beam guardrail. Work will also include replacement of 4 culverts and 9 drainage inlets, and the extension of 3 culverts.

Drainage work includes the drainage system at 41.83, which conveys water from a private driveway and the highway, and requires frequent maintenance during rain events. Using a drainage easement, the two inlets up the driveway will be replaced, and the existing slot drain and culvert will be abandoned and replaced by two new culverts. Erosion has been occurring below the outlet at culvert 42.11. To prevent further erosion, which could compromise the highway, the existing failing downdrain will be removed and replaced with a rock lined ditch and ¼ ton RSP. The existing failing culvert will also be replaced. Below many of the existing cutslopes an inboard ditch carries storm water between culvert inlets. An additional 2 feet of widening beyond the shoulder is included to provide space for this water.

Construction of the new shoulders will include excavation of existing material, and placement of new structural section. The new shoulder structural section will consist of 0.65 ft of Class 2 Subbase, 0.75 ft of Class 2 Base, 0.45 ft of hot mix asphalt (Type A). A layer of geosynthetic pavement interlayer (GPI) will be used at the pavement joint, where the new meets the existing. A new aggregate rubber chip seal will be placed from edge of pavement to edge of pavement to provide a fresh surface for restriping. Prior to the new chip seal placement, the existing rumblestrip will be ground out and paved over. After chip sealing, the centerline rumblestrips will be re-installed along the new alignment. New edgeline rumblestrip will also be ground into place.

The new metal beam guardrail from PM 42.07 to PM 42.30 will include both standard sections, and 7' post segments in narrow roadway locations. New cantilevered and 7' post metal beam guardrail will be installed to span the inlet and outlet of the 6 ft box culvert at PM 41.79.

	<p><b>PROJECT STUDY REPORT</b>  <b>COLLISION REDUCTION</b>                  Safety Improvements</p>	
Navarro Ridge Shoulder Widening		01-MEN-1-41.8/42.3 01-0C550K 0112000300 201.010 January, 2016

01 - MEN - 1 - PM 41.8/42.3

**1. INTRODUCTION**

**Project Description:**

The shoulder widening project is located in Mendocino County near Albion on MEN 1 from PM 41.77 to 42.33, from 1.5 miles north of the junction of SR 128 and SR 1 to 0.1 miles south of Navarro Ridge Road. See Attachment A for the Location Map.

The scope of work proposes to widen existing shoulders in both directions to 4 ft, install edgeline rumblestrip, perpetuate the existing centerline rumblestrip, and install metal beam guardrail. Work will include replacement of four culverts and nine drainage inlets, and the extension of three culverts. See Attachment B for the Typical Cross Section and Attachment C for the Layouts.

The project was initiated by the District 1 Traffic Safety Office in response to a high incidence of run-off-road collisions. A subsequent review of the collision history indicated that a combination of shoulder widening (existing shoulder widths average 0-1 feet) and rumble strips would address the run off the road collisions by providing recovery area and audible warning when leaving the travel way or encroaching into opposing traffic.

<b>Project Limits</b> (Dist., Co., Rte., PM)	01-MEN-1 PM 41.77/42.33
<b>Number of Alternatives:</b>	Two (Including No Build)
<b>Alternative Recommended for Programming:</b>	The Build Alternative



**Project Description Continuation for the Navarro Ridge Safety Project**

**01-0C550**

The proposed project would make safety improvements and perform drainage rehabilitation on State Route 1 (SR 1) between post miles (PM) 41.8 and 42.3 in Mendocino County, just south of Navarro Ridge Road (Vicinity Map, attached). The project is being proposed to reduce the number and severity of collisions by improving roadway geometry, increasing shoulder widths to current standards, and installing rumble strips and a Midwest Guardrail System (MGS). A ten-year review of this road segment shows 20 collisions, 10 of which were "run off the road" accidents resulting in one fatality. 13 of these accidents occurred in the 5-year period from 7/1/2010 to 6/8/2015. Shoulder widening and edgeline rumble strips are needed to provide a recovery area for errant vehicles and reduce the frequency of these types of accidents. The existing shoulders are 0 feet in width. Work would include widening both lanes to 12 feet, widening both road shoulders to 4 feet, and improving the super elevation of the road surface to current standards. The construction of the new shoulders will require excavation of material from the east side of the road and placement of a new structural section. Below many of the existing cutslopes, an inboard ditch carries storm water between culvert inlets. An additional 2 feet of widening beyond the shoulder is included to provide space for this water. Draft plans (Site Plan) are attached to this project description.

There are 7 culverts within the project limits including one 6-foot box culvert. The proposed work includes installing four new culverts, replacing three existing culverts, installing nine new drainage inlets (DIs), removing nine existing DIs, and extending 5 culverts. An additional 2 feet of widening beyond the new inboard shoulder has been included in the project to provide space for a new inboard ditch to carry storm water to the replaced culverts in the project area. Erosion has been occurring below the culvert outlet at PM 42.11. The existing culvert at this location is failing and will be replaced. The downdrain will be also be removed and replaced with a rock-lined ditch and ¼ ton rock slope protection (RSP).

The new MGS installed from PM 42.11 to PM 42.30 will include both standard sections and 7-foot post segments in narrow roadway locations. Also, a new Omit One Post (MGS) will be installed from PM 41.78 to PM 41.80 to span the inlet and outlet of the 6-foot box culvert on both sides of State Route (SR) 1. The MGS to be installed within the project limit will be treated with a light-brown stain to reduce glare and to blend the MGS into the visual character of the natural landscape.

65 trees will be removed as part of the proposed project. These trees are Monterey or Bishop Pine and are primarily located on the west side of SR 1. 45 of the trees are alive (12 Bishop pine, 32 Monterey pine, 1 Douglas fir), 15 are dead (most likely Monterey pine) and 15 are stumps. The living trees appear to be a windbreak or other planted stand and do not represent prime habitat.

The anticipated traffic control measures are reversing traffic control, moving lane closure, and shoulder closure. One-lane closure is permitted within the project limits. A minimum of 12 feet of paved roadway must be open for use by public traffic. Bicyclists will be accommodated through the work zone. Signage will be used to alert vehicle operators to the possible presence of bicyclists. The estimated maximum delay during one-way reversing traffic control will be 10 minutes. Access to side roads and residences will be maintained at all times.

All work would occur within the State Right of Way. Approximately 1.6 acres of additional Right of Way will be acquired on the east side of SR 1 throughout the project area to accommodate the additional road shoulder and inboard ditch width. Biological, cultural, visual, water quality, and hazardous materials reviews have been completed.

**Avoidance and Minimization Measures**

The following section provides a list of project features, standard practices, and best management practices (BMPs) that are included as part of the project description. These measures are provided in the Natural Environment Study (NES), the NES Addendum, and ESHA report with additional details and These avoidance and minimization measures are prescriptive and sufficiently standardized to be generally applicable, and do not require special tailoring to a project situation. These are general

- **AS-1:** To protect migratory and nongame birds, their occupied nests and eggs, nesting prevention measures would be implemented. Vegetation removal would be restricted to the period outside of the bird breeding season (September 1 through February 28), or if vegetation removal is required during the breeding season, a nesting bird survey would be conducted within one week of removal by a qualified biologist. If an active nest were located, the biologist would coordinate with the California Department of Fish and Wildlife (CDFW) to establish appropriate species-specific buffer(s) and any monitoring requirements. The buffer would be delineated around each active nest, and construction activities would be excluded from these areas until birds have fledged, or the nest is determined to be unoccupied.

**Invasive Species**

- **IS-1:** To prevent the spread of invasive plant species in disturbed soil after construction, all disturbed areas would be seeded with native herbaceous species and weed-free mulch would be applied.
- **IS-2:** Construction equipment would be inspected and cleaned to remove invasive species and/or pathogens before being brought to the project site and prior to removal from the project area.

- Areas for Contractor Use have been identified on the north end of the project in existing pullouts on the west and east side of SR 1. No other areas may be used for equipment/materials staging and storage for this project. Maps showing these areas are provided in the attached NES.
- MGR installed at the site will be stained light-brown to reduce glare and to blend the MGS into the visual character of the natural landscape, as described in the Visual Impact Assessment (VIA) developed for this project and attached to this CE.

**Standard Specifications**

- Standard Specification for Slopes – SSP 19-2.03G Slopes. For new cut slopes steeper than 2:1, roughen slope to help ensure the success of hydroseeding work as described in the SSP.
- Standard Specification for Excavation, Handling, and Placing Topsoil – SSP 19-2.03D(2). Provides guidelines for storing and placing native topsoil in such a way as to preserve the seedbank and reduce impacts to areas subject to soil grading and removal.
- Standard Specification for Earth Material Containing Lead – SSP 7-1.02K(6)(j)(iii) Earth Material Containing Lead. Compliance with the SSP and a Lead Compliance Plan will be required.
- Standard Specifications for Air Quality – SSP 14-9.02 (Air pollution control) & SSP 14-9.03 (Dust Control).
- Standard Specifications for Noise Control – SSP 14-8.02A Noise Control

COUNTY OF MENDOCINO  
DEPT OF PLANNING AND BUILDING SERVICES  
120 WEST FIR STREET  
FORT BRAGG, CA 95437  
Telephone: 707-864-5379  
FAX: 707-861-0407  
info@co.mendocino.ca.us  
www.co.mendocino.ca.us/planning

Case No(s): CCP 2019-0024  
CDF No(s):  
Date Filed: 07-01-2019  
Fee: \$6,250.00  
Receipt No: 2019-033025  
Received by: Julia Kiva King  
Office Use Only

**COASTAL ZONE APPLICATION FORM**

**APPLICANT**  
Name: Department of Transportation, District 1  
Address: 1656 Union Street  
City: Eureka State: CA Zip Code: 95501 Phone: (707)441-5602

**PROPERTY OWNER**  
Name: Same as Applicant  
Address: Same as Applicant  
City: Same as Applicant State: Same as Applicant Zip Code: Same as Applicant Phone: Same as Applicant

**AGENT**  
Name: Frank Demling  
Address: 1656 Union Street  
City: Eureka State: CA Zip Code: 95501 Phone: (707)445-8554

**PARCEL SIZE**  Requires Fee  Adjoins  
NA

**STREET ADDRESS OF PROJECT**  
Highway 1 from Post Mile 41.80 to 42.30

**ASSESSOR'S PARCEL NUMBER(S)**

I certify that the information submitted with this application is true and accurate.  
Signature of Applicant/Agent: [Signature] Date: 6/27/19 Signature of Owner: \_\_\_\_\_ Date: \_\_\_\_\_



**CATEGORICAL EXEMPTION/CATEGORICAL EXCLUSION DETERMINATION FORM**

01-MEN-1 District/County/Route:	41.8/42.3 PM/PM:	01-0C550	<b>N/A</b> Federal-Aid Project Number
<b>PROJECT DESCRIPTION:</b> <i>(Briefly describe project including need, purpose, location, limits, and right-of-way requirements.)</i>			
<p>The proposed project would make safety improvements and perform drainage rehabilitation on State Route 1 (SR 1) between post miles (PM) 41.8 and 42.3 in Mendocino County. The project is being proposed to reduce the number and severity of collisions by improving roadway geometry, increasing shoulder widths, and installing rumble strips and a Midwest Guardrail System (MGS). Work would include widening both lanes to 12 feet, widening both road shoulders to 4 feet, and improving the super elevation of the road surface. The construction of the new shoulders will require excavation of material from the east side of the road and placement of a new structural section.</p> <p align="center"><b>See next page for additional project information.</b></p>			

01-MEN-1 District/County/Route:	41.8/42.3 PM/PM:	01-0C550	<b>N/A</b> Federal-Aid Project Number
Continued from page 1:			
<p>There are eight culverts within the project limits, including one six-foot box culvert. The proposed work includes installing four new culverts, replacing three existing culverts, abandoning one culvert, installing nine new drainage inlets (DIs), removing six existing DIs, abandoning two existing DI's, and extending five culverts. An additional two feet of widening beyond the new inboard shoulder has been included in the project to provide space for a new inboard ditch to carry storm water to the replaced culverts in the project area. Erosion has been occurring below the culvert outlet at PM 42.11. The existing culvert at this location is failing and will be replaced. The downdrain will be also be removed and replaced with a rock-lined ditch and rock slope protection. The new MGS will be installed from PM 42.11 to 42.3 along the southbound lane and from 41.78 to 41.8 on both sides of the road and will be treated with a light-brown stain to reduce glare and to blend the MGS into the visual character of the natural landscape.</p> <p>Trees in the clear recovery zone will be removed as part of the proposed project primarily located on the west side of SR 1. All work would occur within the State Right of Way. Approximately 1.6 acres of additional Right of Way will be acquired on the east side of SR 1 throughout the project area to accommodate the additional road shoulder and inboard ditch width. Biological, cultural, visual, water quality, and hazardous materials reviews have been completed.</p> <p><b>The following measures have been included as part of the project:</b></p> <ul style="list-style-type: none"> <li>• Sensitive habitat areas identified in the Natural Environment Study (NES) and Environmentally Sensitive Habitat Areas (ESHA) Analysis will be delineated by High Visibility Fencing (HVF) and avoided during construction.</li> <li>• Areas for Contractor Use have been identified on the north end of the project in existing pullouts on the west and east side of SR 1. No other areas may be used for equipment/materials staging and storage for this project.</li> <li>• MGS installed at the site will be stained light-brown to reduce glare and to blend the MGS into the visual character of the natural landscape.</li> <li>• Standard Specification for Slopes – <b>SSP 19-2.03G Slopes</b>. For new cut slopes steeper than 2:1, roughen slope to help ensure the success of hydroseeding work as described in the SSP.</li> <li>• Standard Specification for Earth Material Containing Lead – <b>SSP 7-1.02K(6)(J)(III) Earth Material Containing Lead</b>. Compliance with the SSP and a Lead Compliance Plan will be required.</li> <li>• Standard Specifications for Air Quality – <b>SSP 14-9.02 (Air pollution control) &amp; SSP 14-9.03 (Dust Control)</b>.</li> <li>• Standard Specifications for Noise Control – <b>SSP 14-8.02A Noise Control</b></li> </ul>			



**Memorandum**

*Making Conservation  
a California Way of Life.*

**To:** STEPHEN UMBERTIS  
Associate Environmental Planner, E4  
Caltrans North Region Environmental Planning  
D1, Eureka

**Date:** November 28, 2018

**File:** 01-MEN-1-PM 41.80 / 42.30  
EA 01-0C550  
Navarro Ridge Safety Project

**From:** DESIREE DAVENPORT  
Associate Environmental Planner, Natural Sciences, E4  
Caltrans North Region Environmental Planning  
D1, Eureka

**Subject:** NES ADDENDUM FOR NAVARRO RIDGE SAFETY PROJECT

A temporary construction easement from private landowners for the culvert outlet work would be required for the proposed build alternative. It is anticipated no other temporary or permanent right of way would be required. Right of way fencing that has been undermined in an area of erosion would be reconstructed after construction is complete.

The following section provides a list of project features, standard practices, and best management practices (BMPs) that are included as part of the project description. These avoidance and minimization measures are prescriptive and sufficiently standardized to be generally applicable, and do not require special tailoring to a project situation. These are general measures that result from laws, permits, guidelines, and resource management plans that are relevant to the project. They contain refinements in planning policies and implementing actions. These practices predate the project's proposal and apply to all similar projects. For this reason, these measures and practices do not qualify as project mitigation, and the effects of the project are analyzed with these measures in place. Any project-specific mitigation measures that would be applied to reduce the effects of project impacts are listed in relevant sections of Chapter 4 of the NES (Caltrans 2017).

Standard measures relevant to the protection of natural resources deemed applicable to the proposed project include the following:

*Water Quality and Storm Water Runoff*

**WQ-1:** Construction site BMPs would follow the latest edition of the Construction Site Best Management Practices Manual (Caltrans 2017b) to control and minimize the impacts of construction-related activities, materials and pollutants on the watershed.

**WQ-2:** The project would comply with Caltrans Standard Specifications for Water Pollution Control and Job Site Management (Caltrans 2015). Caltrans' Standard Specifications require the contractor to submit a Water Pollution Control Plan for projects with a disturbed soil area (DSA) of less than one acre for review and approval by the resident engineer. The Water Pollution Control Plan would implement storm water and water pollution control training, routine BMP inspections, spill prevention and control, materials and waste management and non-storm water management.

*Wetlands and Other Waters*

**WW-1:** The contractor would be required to place high visibility fencing (HVF) along the boundaries of all riparian, wetland or other environmentally sensitive areas adjacent to the project footprint. After construction is complete, the contractor would be required to remove and dispose of HVF at a nearby recycling facility.

**WW-2:** Revegetation would take place on-site after construction to address permanent and temporary impacts to other waters of the U.S. and associated riparian areas resulting from the proposed project. All disturbed soil areas would be planted with regionally appropriate native plants. Caltrans would remove non-native and invasive plants within disturbed soil areas as needed during the plant establishment period. Planting ratios would be determined following updated guidance from USACE, NCRWQCB, and County of Mendocino Department of Planning and Building (MDP&BS). A Revegetation Plan would be submitted during the permitting phase of the project to address all project requirements.

**WW-3:** The construction footprint would be reduced to the maximum extent feasible.

*Natural Communities*

**NC-1:** After all construction materials are removed, the project area would be revegetated. Replanting would be subject to a plant establishment period as defined by project permits, which would require Caltrans to adequately water plants, replace unsuitable plants, and control pests.

Caltrans would implement a program of invasive weed control in all areas of soil disturbance caused by construction to improve habitat for native species in and adjacent to disturbed soil areas within the project limits.

**NC-2:** The contractor would be required to place HVF along the boundaries of riparian, wetland or other environmentally sensitive areas on land to avoid impacts to sensitive habitats that occur adjacent to the project footprint. After construction is complete, the contractor would be required to remove and dispose of HVF at a nearby recycling facility.

*Animal Species*

**AS-1:** To protect migratory and nongame birds, their occupied nests and eggs, nesting prevention measures would be implemented. Vegetation removal would be restricted to the period outside of the bird breeding season (September 1 through February 28), or if vegetation removal is required during the breeding season, a nesting bird survey would be conducted within one week of removal by a qualified biologist. If an active nest were located, the biologist would coordinate with the California Department of Fish and Wildlife (CDFW) to establish appropriate species-specific buffer(s) and any monitoring requirements. The buffer would be delineated around each active nest, and construction activities would be excluded from these areas until birds have fledged, or the nest is determined to be unoccupied.

*Invasive Species*

**IS-1:** To prevent the spread of invasive plant species in disturbed soil after construction, all disturbed areas would be seeded with native herbaceous species and weed-free mulch would be applied.

**IS-2:** Construction equipment would be inspected and cleaned to remove invasive species and/or pathogens before being brought to the project site and prior to removal from the project area.



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### *Wetlands and Other Waters*

**WW-1:** The contractor would be required to place high visibility fencing (HVF) along the boundaries of all riparian, wetland or other environmentally sensitive areas adjacent to the project footprint. After construction is complete, the contractor would be required to remove and dispose of HVF at a nearby recycling facility.

**WW-2:** Revegetation would take place on-site after construction to address permanent and temporary impacts to other waters of the U.S. and associated riparian areas resulting from the proposed project. All disturbed soil areas would be planted with regionally appropriate native plants. Caltrans would remove non-native and invasive plants within disturbed soil areas as needed during the plant establishment period. Planting ratios would be determined following updated guidance from USACE, NCRWQCB, and County of Mendocino Department of Planning and Building (MDP&BS). A Revegetation Plan would be submitted during the permitting phase of the project to address all project requirements.

**WW-3:** The construction footprint would be reduced to the maximum extent feasible.

### *Natural Communities*

**NC-1:** After all construction materials are removed, the project area would be revegetated. Replanting would be subject to a plant establishment period as defined by project permits, which would require Caltrans to adequately water plants, replace unsuitable plants, and control pests.

Caltrans would implement a program of invasive weed control in all areas of soil disturbance caused by construction to improve habitat for native species in and adjacent to disturbed soil areas within the project limits.

**NC-2:** The contractor would be required to place HVF along the boundaries of riparian, wetland or other environmentally sensitive areas on land to avoid impacts to sensitive habitats that occur adjacent to the project footprint. After construction is complete, the contractor would be required to remove and dispose of HVF at a nearby recycling facility.

### *Animal Species*

**AS-1:** To protect migratory and nongame birds, their occupied nests and eggs, nesting prevention measures would be implemented. Vegetation removal would be restricted to the period outside of the bird breeding season (September 1 through February 28), or if vegetation removal is required during the breeding season, a nesting bird survey would be conducted within one week of removal by a qualified biologist. If an active nest were located, the biologist would coordinate with the California Department of Fish and Wildlife (CDFW) to establish appropriate species-specific buffer(s) and any monitoring requirements. The buffer would be delineated around each active nest, and construction activities would be excluded from these areas until birds have fledged, or the nest is determined to be unoccupied.

### *Invasive Species*

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Navarro Ridge Safety Project



Natural Environment Study

State Route 1 Mendocino County  
 01- MEN 1 PM 41.80 - PM 42.30  
 EA: 01-0C550  
 EFIS: 0112000300  
 September 2017



1.2 Project Description

Two alternatives have been proposed for this project, a build and a no-build alternative.

1.2.1 Build Alternative

The scope of work includes the following components (see Appendix B for preliminary plans):

- widen existing shoulders in both directions to 4-feet,
- install edge line rumble strip,
- install guardrail (Midwest Guardrail System [MGS]),
- replace 4 culverts, 9 drainage inlets and extend 3 culverts, and
- remove 5 trees from within the clear recovery zone on the west side of the highway (PM 41.89-42.06).

The existing right-of-way boundary will be widened to accommodate the cut/fill required for shoulder widening, safety and drainage improvements. Cut and fill slopes will be required to accommodate the increase in shoulder width. A 4-foot wide fill section at 5% slope with a Type E dyke will be required. The catch slope will most likely be 2:1 going downhill. Construction of

new shoulders will include excavating existing material and placing a new shoulder structural section that will consist of 0.65 feet of Class 2 Subbase, 0.75 feet of Class 2 Subbase, and 0.45 feet of hot asphalt mix (Type A). A layer of geosynthetic pavement interlayer will be used at the pavement joints where the new section meets existing. A new aggregate rubber chip seal will be placed from the edge of pavement to provide a fresh surface for restriping. Prior to the new chip seal placement, the existing rumblestrip will be ground out and paved over. After chip sealing, the centerline rumblestrips will be reinstated along the new alignment. New edgeline rumblestrip will also be ground into place.

The new MGS from PM 42.07 to 42.3 will include both standard sections, and 7-ft post segments in narrow roadway locations. New cantilevered and 7-ft post MGS will be installed to span the inlet and outlet of the existing 6-ft box culvert at PM 41.79. Below many of the existing cut slopes, an inboard ditch carries storm water between culvert inlets. The proposed project includes an additional 2-ft of widening beyond the shoulder to provide space for this water.

Culvert and drainage components are summarized in Table 1. The proposed drainage work at PMs 41.83 and 41.84 involves replacing the two drainage inlets (DI) located on either side of the private driveway, abandoning the existing "Y" slot drain and culvert, and replacing it with two new culverts. Flow patterns will remain the same. However, rather than collecting flow into a "Y" slot drain that is difficult to maintain (and a safety hazard when clogged), surface runoff will be collected into two new culverts that will form an "L" spanning the driveway then across the highway. The outfall at PM 41.83 will be at the same location as the existing culvert.

Table 1. Drainage facilities in the project area.

PM	Existing Type	Proposed Activity
41.79	6-ft box culvert	None
41.83	18-in culvert with a "Y" slot drains and drainage inlets (DI)s on either side of existing driveway	Replace with 24-in culvert, install new DI south of driveway
41.84	None	Install 24-in culvert across driveway and new DI north of driveway
41.95	18-in culvert	Extend inlet end and replace the DI
41.98	18-in culvert	Extend inlet end and replace the DI
42.02	24-in culvert	Replace with 24-in culvert; extend inlet end and replace the DI
42.11	18-in culvert and 40-ft down-drain	Replace with 18-in culvert and DI; extend inlet end, remove down-drain, line gully with 1/4 ton rock slope protection
42.26	18-in culvert	Extend inlet end and replace the DI

Construction is programmed for 2020. Equipment needed to perform the work includes support vehicles, dump trucks, pickup trucks, hauling trucks, backhoe, trencher, pile driver, drilling rigs/augers, paver, rollers, concrete saw, jackhammer, generators, grinders, compressors, concrete saws, other handheld power tools, and drums to store debris from surface preparation work. For work on the drainage culverts, equipment will operate from the road prism or within construction limits. A staging area for construction has been identified at the north end of the project, in an existing pullout located on the west side of the highway. This is an area that was formerly the main highway alignment, abandoned several years ago for a curve correction project associated with Navarro Ridge Road. Other areas that may also be used for

construction parking include the gravel pullout at the northwest corner of Navarro Ridge Road and SR 1.



## Memorandum

*Making Conservation  
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**To:** STEPHEN UMBERTIS  
Associate Environmental Planner, E4  
Caltrans North Region Environmental Planning  
D1, Eureka

**Date:** November 28, 2018

**File:** 01-MEN-1-PM 41.80 / 42.30  
EA 01-0C550  
Navarro Ridge Safety Project

**From:** DESIREE DAVENPORT  
Associate Environmental Planner, Natural Sciences, E4  
Caltrans North Region Environmental Planning  
D1, Eureka

**Subject:** NES ADDENDUM FOR NAVARRO RIDGE SAFETY PROJECT

### Project Description Updates

The current project description overall would involve installing four new culverts, replacing three existing culverts, installing nine new drainage inlets (DIs), removing nine DIs, and extending five culverts (Updated Table 1). At post mile (PM) 41.83, the existing 18-inch culvert would be abandoned, the existing slot drain and DIs would be removed, four new DIs would be installed and three new 18- to 24-inch culverts would be installed. This change is not expected to increase impact estimates to jurisdictional waters because it is not creating additional temporary impact to currently culverted waters. See Attachment G for updated project plans.

New Midwest Guardrail System (MGS) would be installed from PM 42.11 to PM 42.30 and would include both standard sections and 7-foot post segments in narrow roadway locations. This proposed section of MGS has been reduced from the original project description that included MGS from PM 42.07 to PM 42.30. Also, a new Midwest Guardrail System (MGS) would be installed from PM 41.78 to PM 41.80 to span the inlet and outlet of the 6-foot box culvert on both sides of State Route (SR) 1. MGS installed within the project limits would be treated with a light-brown stain to reduce glare and to blend the MGS into the visual character of the natural landscape.

Anticipated traffic control measures include reversing traffic control, moving lane closure, and shoulder closure. One-lane closure is permitted within the project limits. A minimum of 12 feet of paved roadway must remain open for use by public traffic. Bicyclists would be accommodated through the work zone. Signage would be used to alert vehicle operators to the possible presence of bicyclists. The estimated maximum delay during one-way reversing traffic control would be 10 minutes. Access to side roads and residences would be maintained at all times.

The contractor would remove all vegetation and debris within the right of way (ROW) and within temporary construction easements as specified, except for environmentally sensitive habitat areas (ESHAs) that require preservation. ESHAs would be protected from injury by the contractor with the use of high visibility fencing (HVF). One bishop pine (*Pinus muricata*) under 6 inches diameter-at-breast-height (DBH) is within the drainage and would have to be removed to install the new culvert. In compliance with the Migratory Bird Treaty Act, vegetation clearing would be limited to September 1<sup>st</sup> to February 28<sup>th</sup> or if vegetation removal is required during the breeding season, a nesting bird survey would be conducted by a qualified biologist within one week of removal. Vegetation that is cleared and grubbed may be collected and processed into duff by grinding or chipping. Duff may be stockpiled until placed on the planned revegetation areas. Alternately, Type D Erosion Control may be used which would be a combination of hydroseeding, straw, and fiber application. Access and staging areas would be cleared as necessary to move and store material and equipment around the project site. Sixty two (62) trees are to be removed on State right of way and thirteen (13) trees removed on the Temporary Construction Easement (TCE). Of the 75 trees in total, 45 of the trees are alive (12 Bishop pines, 32 Monterey Pines and 1 Douglas-fir), 15 are dead standing trees (most likely Monterey pines), and 15 are stumps. These trees may be removed for construction access and widening activities. Equipment used to clear and grub vegetation would likely include backhoes, chain saws, mowers, chippers, and hand tools. Access roads would likely be graded with a backhoe.

After all construction materials are removed, the site would be restored to a natural setting by grading, placing erosion control, and replanting. Replanting would be subject to a plant establishment period as defined by permits, which would require Caltrans to adequately water plants, replace unsuitable plants, weed, and control pests.



## 01-0C550 Navarro Ridge Safety Improvement Project PM 41.8/42.3

### Project Description

The California Department of Transportation (Department) is proposing to improve the safety of State Route (SR) 1 from 1.5 miles north of the junction of SR 128 and SR 1 to 0.1 mile south of Navarro Ridge Road, in Mendocino County, near Albion.

Caltrans District 1 Traffic Safety Office initiated the subject project in response to a high incidence of run-off-road collisions. A subsequent review of the collision history indicated that shoulder widening would address the run-off-road collisions by providing recovery area when a vehicle is leaving the traveled way. The project also includes replacing the centerline rumble strip as an audible warning to vehicles when encroaching into opposing traffic.

This segment of highway has an actual Fatal collision rate of 18.6 times the Statewide Average (SWA) for similar facilities, an actual Fatal + Injury (F+I) collision rate of 2.0 times the SWA for similar facilities, and actual total collision rate of 1.5 times the SWA for similar facilities. During the most recent 3-year period for which the information is available (January 1, 2016 to December 31, 2018), 3 collisions were reported for the highway segment from PM 41.8 to PM 42.3. The predominant movement preceding collision was run-off-road. There is a need to reduce the frequency of run-off-road collisions within this highway segment

Within the project limits, the facility is a two-lane conventional highway on rolling terrain with 11.5 to 12-foot lanes and 0- to 1-foot shoulders. The posted speed limit is 55 miles per hour. The adjacent land uses are low-density residential and open space rangeland. The project location overlooks the Pacific Ocean, which lies approximately 1,000 feet to the west. A coastal access trail and a public parking area are present within the project limits. The driveway to the coastal access parking area managed by Mendocino Land Trust is across the highway from a private driveway. There are eight culverts within the project limits, including one 6-foot box culvert at PM 41.79, previously used for livestock that would be extended 10.5 feet to the west and 5.5 feet to the east to ensure it continues to function following the proposed shoulder widening at this location. A center-line rumble strip is present throughout this segment (i.e., SR 1 from PM 41.8 to PM 42.3).

Existing horizontal curves do not meet standard radii at several locations throughout the project limits. The project proposes to widen the existing lanes to 12 feet, widen the existing shoulders in both directions to 4 feet, install a Midwest Guardrail System (MGS), improve the superelevation (rate, transition, and runoff), and remove trees. Work will also involve installing two new culverts, replacing four existing culverts, removing one culvert, extending one box culvert, installing five new drainage inlets (DIs), replacing three DIs,

and removing 2 slot drains. In addition, Rock Slope Protection (RSP) will be placed at six outlet locations.

The construction of the new shoulders will involve the excavation of existing material and the placement of a new structural section. The structural section will consist of 1.30 feet of class 2 aggregate base, 0.40 foot of hot mix asphalt (type A), and 0.08 foot of bonded wearing course (BWC-G). The BWC-G layer will help seal longitudinal pavement joints and provide a good surface for receiving new striping. A layer of geosynthetic pavement interlayer (GPI) will be used at the pavement joint where the new structural section meets the existing pavement.

The new MGS installed from PM 42.11 to PM 42.30 will include both standard sections and 7-foot post segments in narrow roadway locations. Also, a new Omit One Post (MGS) will be installed from PM 41.78 to PM 41.80 to span the inlet and outlet of the 6-foot box culvert on both sides of SR 1. The MGS to be installed within the project limit will be treated with a light-brown stain to reduce glare and to blend the MGS into the visual character of the natural landscape.

Erosion has been occurring below the culvert outlet at PM 42.11. To prevent further erosion at this location, which could compromise the highway; the existing down drain, which is failing, will be removed and replaced with a new down drain and RSP. The existing culvert at this location, which is also failing, will be replaced.

The existing cut bank on the east side of SR 1 will be laid back to provide adequate space for the proposed widening. Final cut slopes will not be steeper than 1.1:1 and final fill slopes will not be steeper than 1.4:1. Cut slope heights will be a maximum of 52 feet and fill slope heights will be a maximum of 16 feet. Of the approximately 6,766 cubic yards of material removed from the cut banks, up to 3,462 cubic yards would be used to provide fill for the structural section and shoulder extension on the west side of the road. Approximately 3,300 cubic yards of excess material would be hauled off site to an approved disposal site under the responsibility of the contractor. Below many of the existing cut slopes, an inboard ditch carries storm water between culvert inlets. An additional 2 feet of widening beyond the shoulder is included to provide space for this water.

61 trees in the clear recovery zone west of the highway and 5 trees on the east side will be removed.

The anticipated traffic control measures are reversing traffic control, moving lane closure, and shoulder closure. One-lane closure is permitted within the project limits. A minimum of 12 feet of paved roadway must be open for use by public traffic. Bicyclists will be accommodated through the work zone. Signage will be used to alert vehicle operators to the possible presence of bicyclists. The estimated maximum delay during one-way reversing traffic control will be 10 minutes. Access to side roads and residences will be maintained at all times.

*"Provide a safe, sustainable, integrated and efficient transportation system  
to enhance California's economy and livability"*

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**NOTE:**

FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

- TREE REMOVAL:
  - Cut/Fill Limits within R/W: 62 Nos
  - Within TCE: 13 Nos
  - Total: 75 Nos
- Superelevation has been improved. See superelevation diagram for more details.
- For shoulderbacking, see typical cross sections and Standard Plan P75 Case A.
- All MGS shall be wood posts.
- The MGS to be installed will be treated with a light-brown stain.
- Below many of the existing cut-slopes, an inboard ditch carries stormwater between culvert inlets. An additional 2 feet of widening beyond the shoulder is included to provide space for this water.
- Rumble strips will be installed at the edge of the travel lanes and on the centerline of the roadway.

**PRELIMINARY NOT FOR CONSTRUCTION**

**LEGEND:**

- CUT/FILL Limit
- Proposed R/W
- Existing Centerline
- New Centerline
- New ETW
- New ES
- Existing ETW
- Existing Trees to remove (Yellow trees are located in TCE area)
- Existing R/W
- ESL (Environmental Study Limit)
- MGS (Midwest Guardrail System)
- DI (Drainage Inlet)

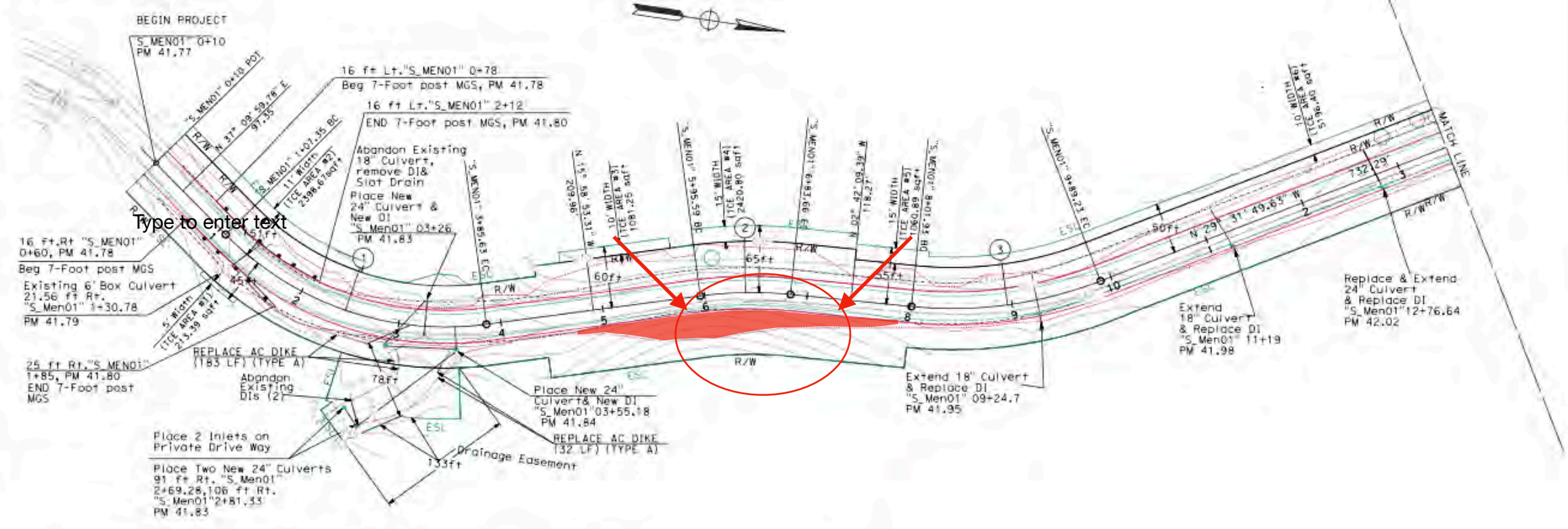
**MEN 1 PM 41.8/42.3**  
**BUILD ALTERNATIVE 1**  
**01-0C550**

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
01	Men	1	41.8/42.3	1	2

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**04 DESIGN**  
 Caltrans



**CURVE DATA**

No. @	R	Δ	T	L	N-COORDINATE	E-COORDINATE
1	300'	53° 08' 53"	150.06'	278.28'	2203121.42	6060707.86
2	380'	13° 16' 44"	44.23'	88.07'	2203510.49	6061303.77
3	400'	26° 49' 40"	95.40'	187.29'	2203591.85	6060519.06

**LAYOUT MAP**

**LAYOUT**  
 SCALE: 1" = 50'

**L-1**

EXHIBIT 19. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022



DIST	COUNTY	ROUTE	PROJECT MILES	SHEET TOTAL
01	Mon	1	41.8/42.3	NO. SHEETS

**DRAFT** XX-KX-20  
REGISTERED CIVIL ENGINEER DATE  
XX-KX-20  
PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SKIPPED COPIES OF THIS PLAN SHEET.

**NOTE:**

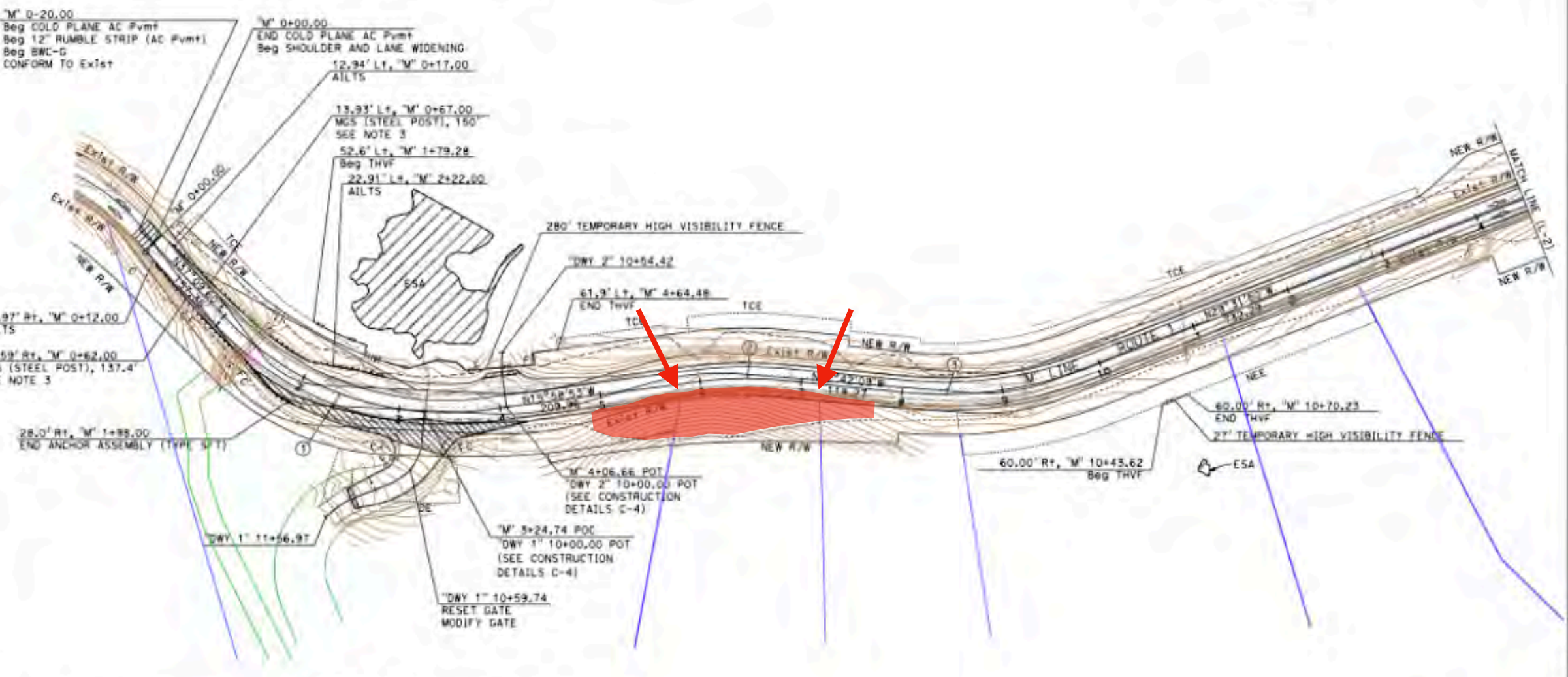
1. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
2. FOR GEOSYNTHETIC REINFORCED EMBANKMENT DETAILS, SEE CONSTRUCTION DETAILS SHEETS C-1 TO C-3.
3. FOR MGS POST OMISSION DETAILS, SEE CONSTRUCTION DETAILS SHEET C-6.

**LEGEND:**

- COLD PLANE AC PAVEMENT
- OBLITERATE SURFACING
- GEOSYNTHETIC REINFORCED EMBANKMENT
- TCE

- ABBREVIATIONS:**
- AULTS ALTERNATIVE IN-LINE TERMINAL SYSTEM
  - BWC-G BONDED WEARING COURSE (GAP GRADED)
  - GRE GEOSYNTHETIC REINFORCED EMBANKMENT
  - DE DRAINAGE EASEMENT
  - NEE NON EXCLUSIVE EASEMENT
  - NEW R/W NEW RIGHT OF WAY

REVISOR: JASON LIU, JON P. MCKEAN, JAMES O. RASMUSSEN  
DESIGNER: JASON LIU, JON P. MCKEAN, JAMES O. RASMUSSEN  
FUNCTIONAL SUPERVISOR: JAMES O. RASMUSSEN  
DEPARTMENT OF TRANSPORTATION  
D3-DESIGN



**CURVE DATA**

No.	R	Δ	T	L	BC	EC
1	300.00'	53°08'53.09"	150.06'	278.28'	"M" 1+07.35	"M" 3+85.63
2	380.00'	13°16'43.92"	44.23'	88.01'	"M" 5+95.59	"M" 6+83.66
3	400.00'	26°49'40.24"	95.40'	187.29'	"M" 8+01.93	"M" 9+89.23

**LAYOUT**  
SCALE: 1" = 50'  
**L-1**

EXHIBIT 20. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022



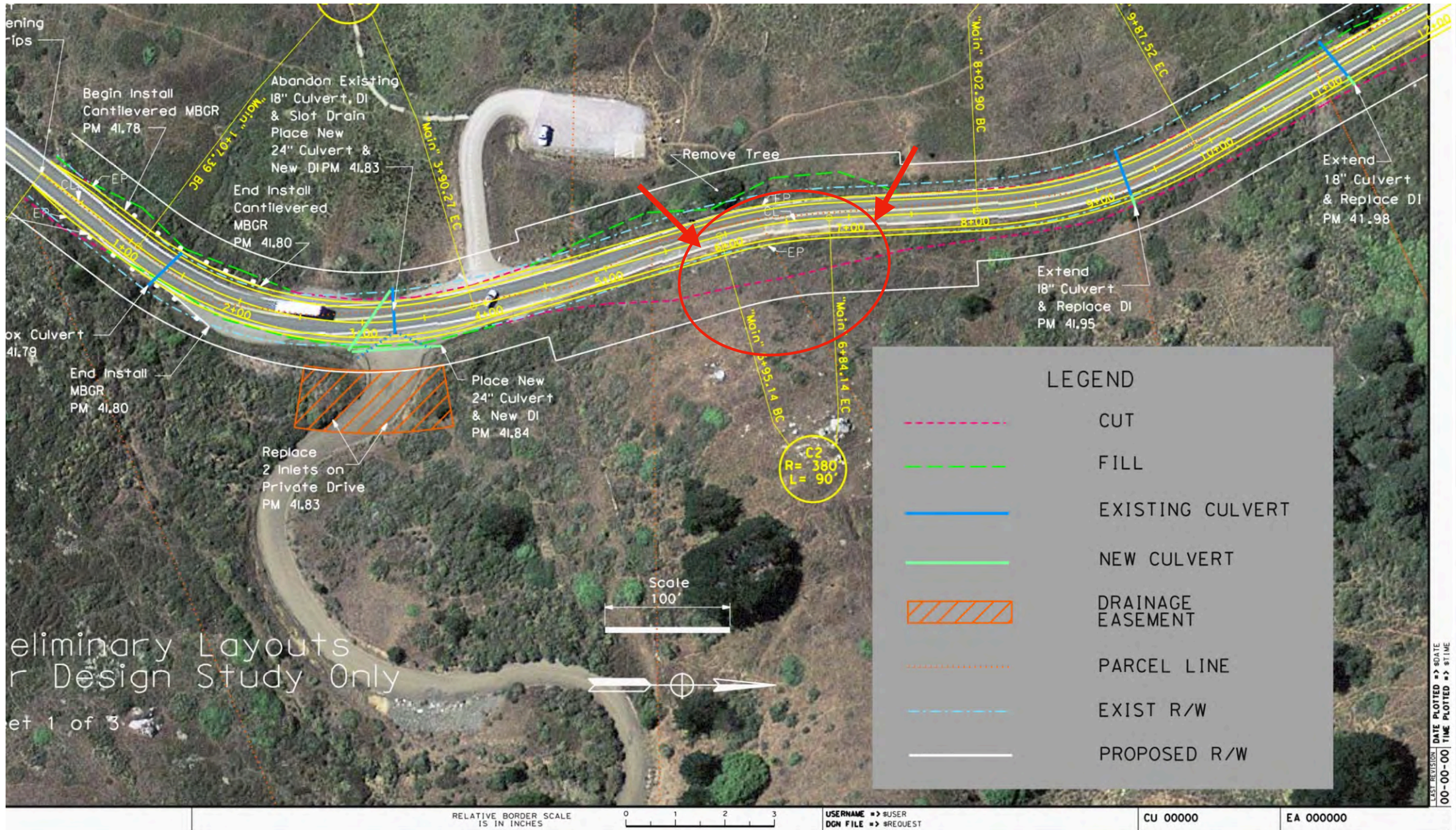


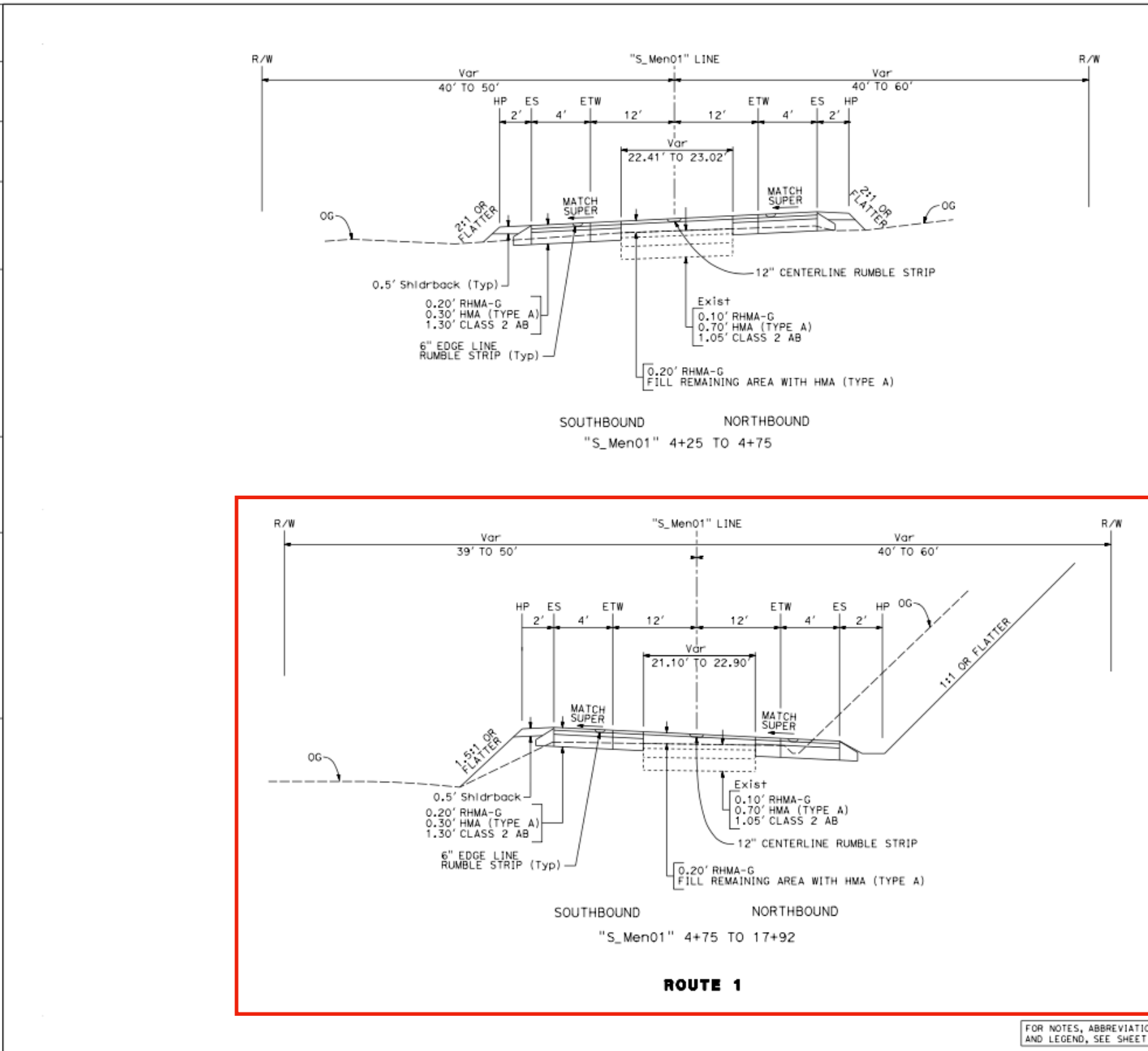
EXHIBIT 21. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022







STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	DESIGN
FUNCTIONAL SUPERVISOR	CHECKED BY
DESIGNED BY	DATE REVISION
REVISOR	DATE REVISION
DATE REVISION	DATE REVISION



DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
01	Men	1	41.8/42.3	3	5

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**TYPICAL CROSS SECTIONS**  
NO SCALE  
**X-3**

FOR NOTES, ABBREVIATIONS AND LEGEND, SEE SHEET X-1

LAST REVISION DATE PLOTTED 03-02-MAR-2019 03-02-19 TIME PLOTTED 09:11:20

EXHIBIT 23. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022



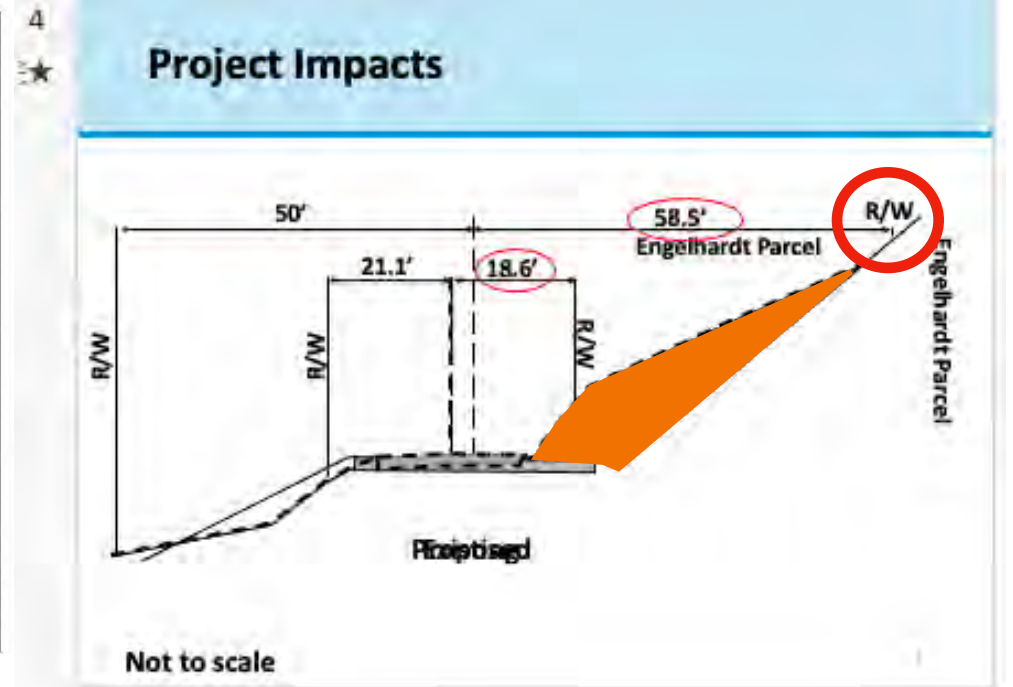
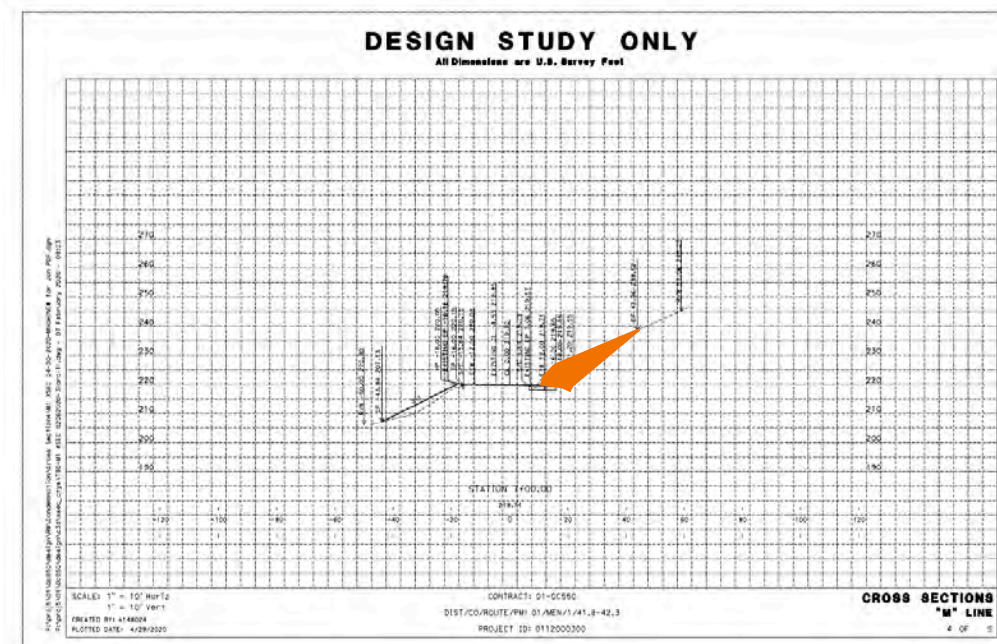
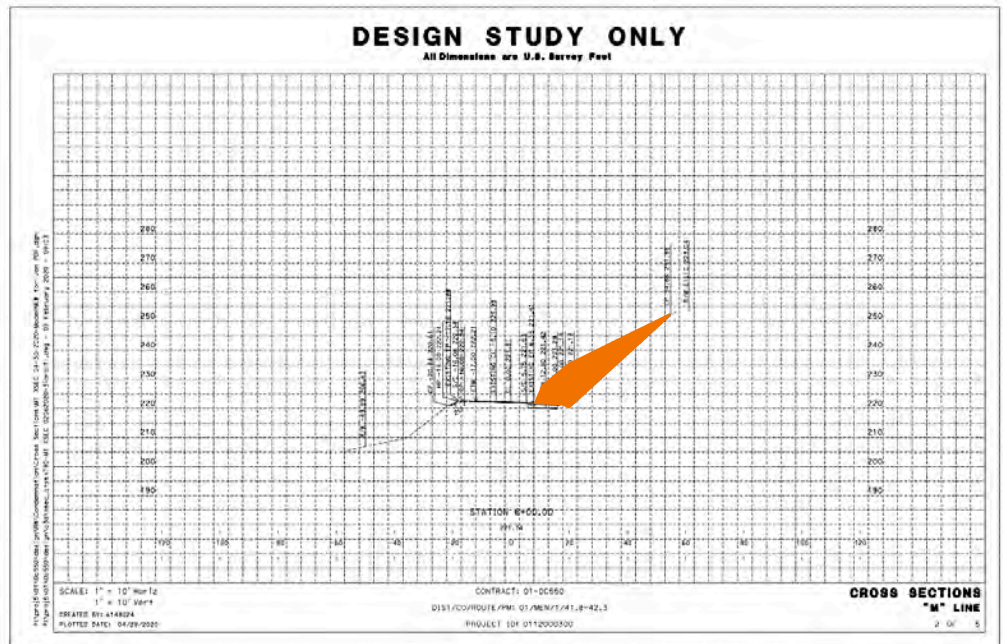
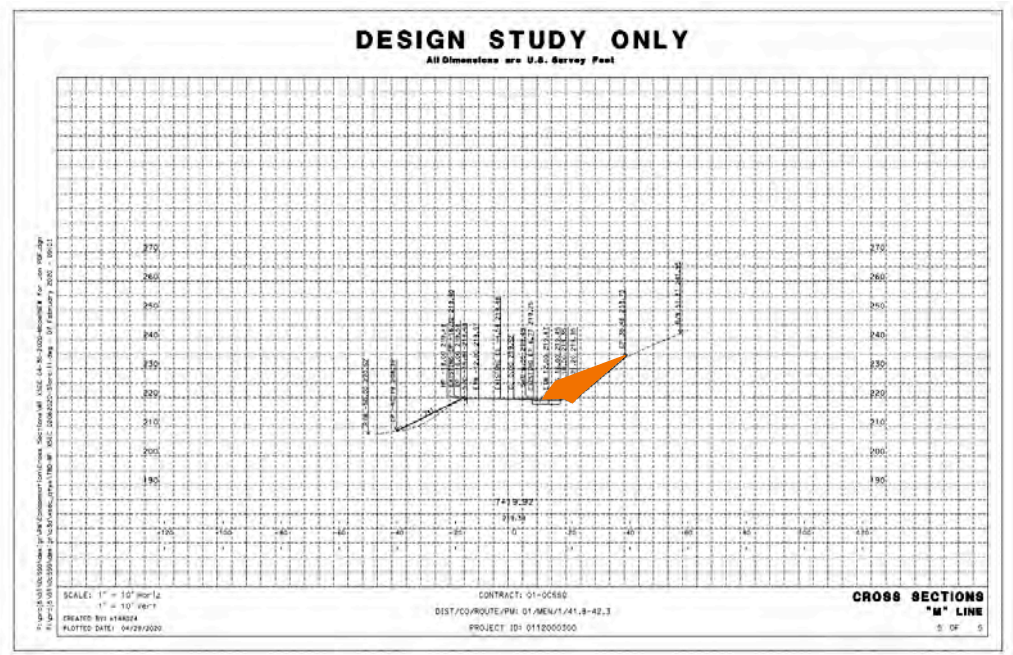
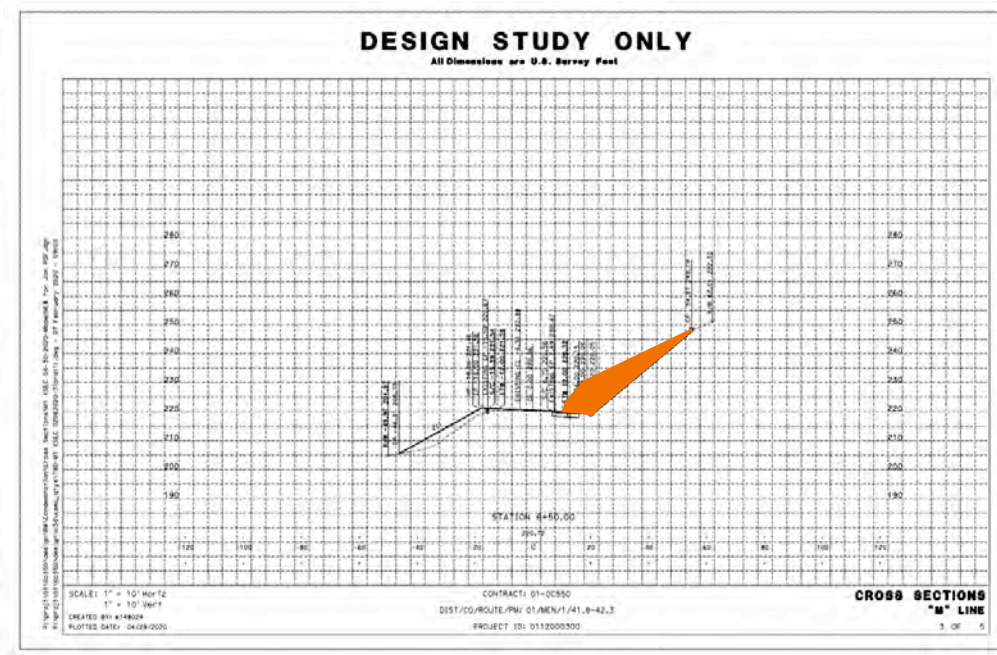
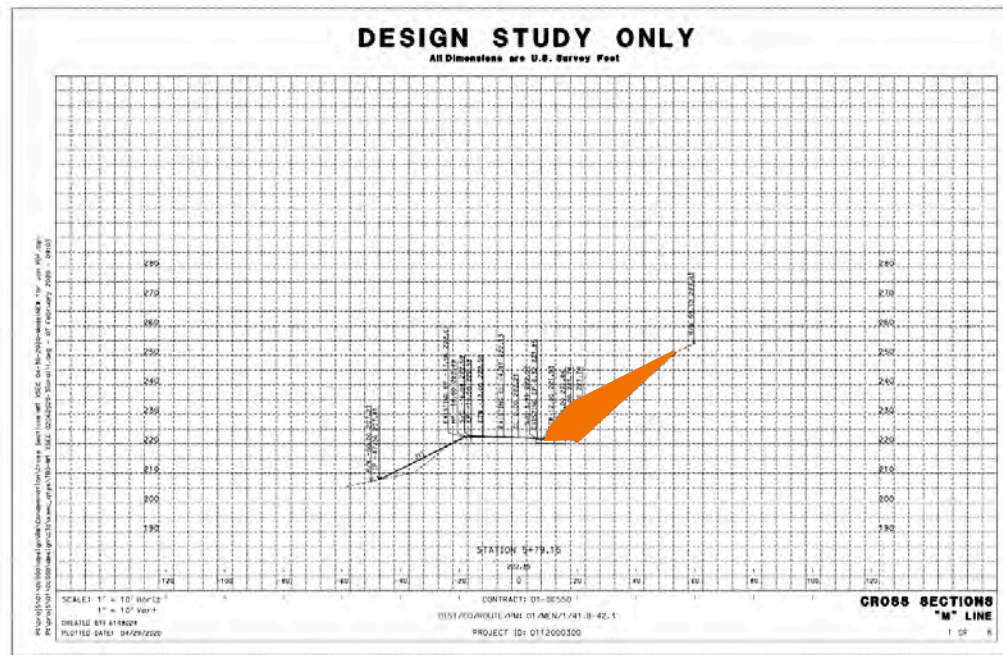






Figure 4. ESHA Map (Middle Extent)

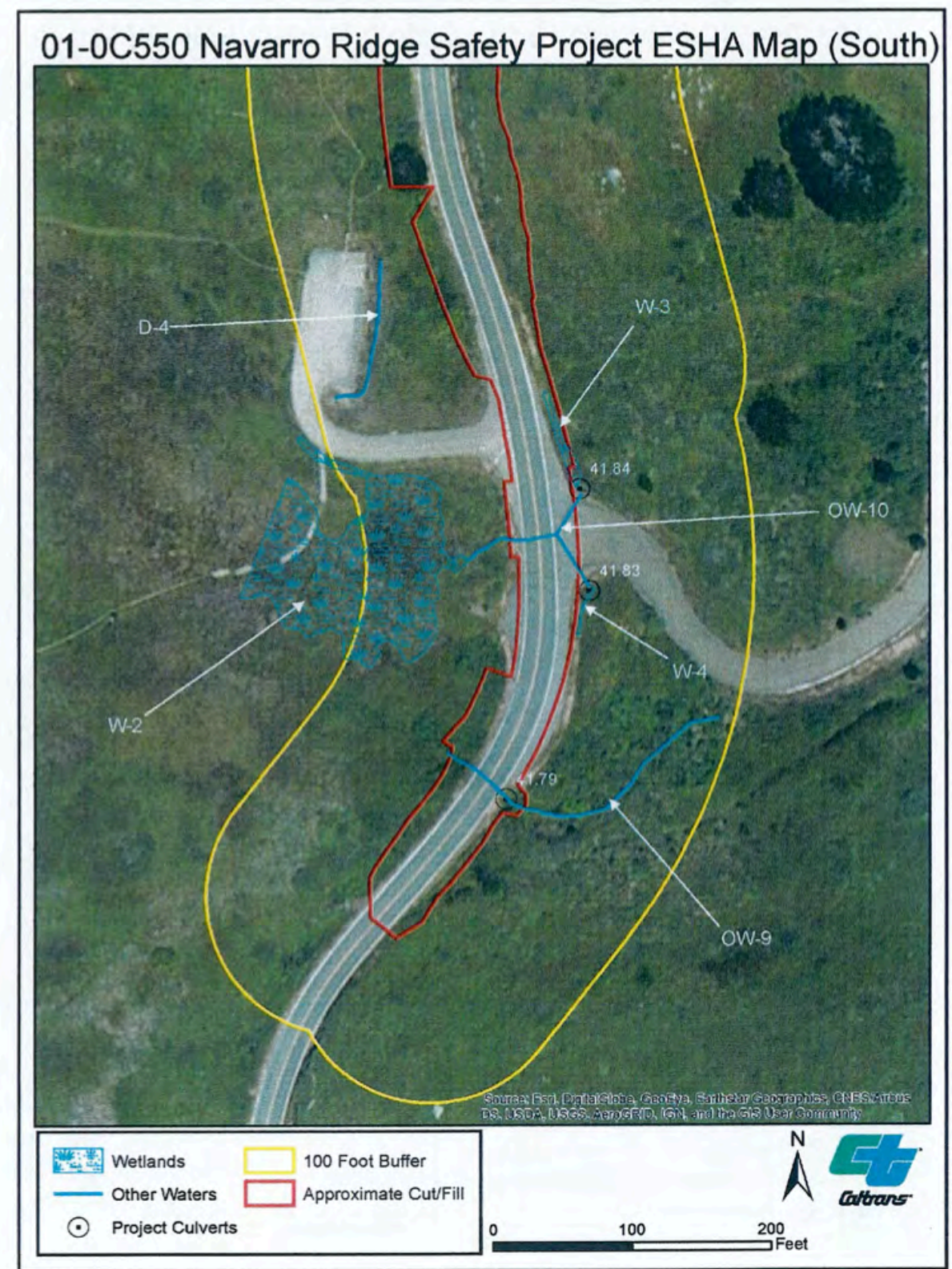


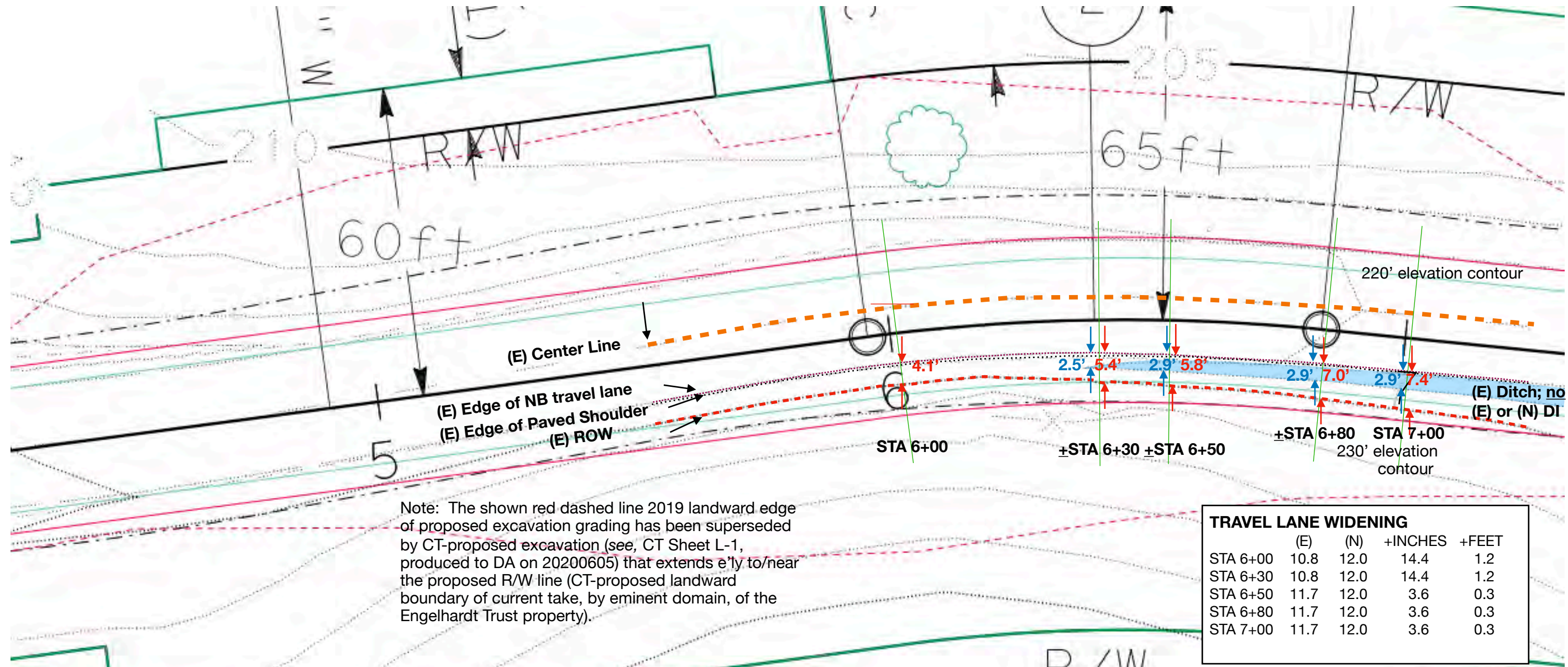
Figure 5. ESHA Map (Southern Extent)





**EXHIBIT 26. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022**





Note: The shown red dashed line 2019 landward edge of proposed excavation grading has been superseded by CT-proposed excavation (see, CT Sheet L-1, produced to DA on 20200605) that extends e'ly to/near the proposed R/W line (CT-proposed landward boundary of current take, by eminent domain, of the Engelhardt Trust property).

TRAVEL LANE WIDENING				
	(E)	(N)	+INCHES	+FEET
STA 6+00	10.8	12.0	14.4	1.2
STA 6+30	10.8	12.0	14.4	1.2
STA 6+50	11.7	12.0	3.6	0.3
STA 6+80	11.7	12.0	3.6	0.3
STA 7+00	11.7	12.0	3.6	0.3

NB AREA DIMENSIONS, 0C550 ON ENGELHARDT TRUST PROPERTY <STA 6 to >STA 7					
	STA 6	+STA 6+30	+STA 6+50	+STA 6+80	STA 7
(E) ETW-(E) E'ly ROW*	4.1'	5.4'	5.8'	7.0'	7.4'
(E) ETW-E'ly DITCH EDGE....	---	2.5'	2.9'	2.9'	2.9'

\* Erroneously claimed by Caltrans, without proof of recorded document, as State property in fee title. The Engelhardt Trust property line (shown in a dashed dark blue line) extends along/near the westerly edge of the Caltrans-shown center line of Highway 1.

**LEGEND:**

- Cut/Fill Limit
- Proposed R/W
- Existing Centerline
- New Centerline
- New ETW
- New ES
- Existing ETW
- Existing Trees to remove (Yellow trees are located in TCE area)
- Existing R/W
- ESL (Environmental Study Limit)
- MGS (Midwest Guardrail System)
- DI (Drainage Inlet)

**EXHIBIT 27. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022.** DA preliminary analysis, Geometry/Dimensions of Highway 1 CL, (Existing [E]) NB ETW, (E) Edge of Paved Shoulder, (E) Drainage Ditch boundaries, (E) ROW. 20200814. Base map sources: Caltrans Layout Map L-1, Project Number & Phase 01120003001, March 13, 2019; Caltrans Layout sheet L-1, file "0C550\_L-1.pdf (page 1 of 1), produced by CT to DA on June 5, 2020 in response to PRAR for current L-1 sheet (site plan) with identified topographic contours.



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**EXHIBIT 27. Dall & Associates, Consultant to Engelhardt Trust, RON C-21939, California Transportation Commission, 20201022.** DA preliminary analysis, Geometry/Dimensions of Highway 1 CL, (Existing [E]) NB ETW, (E) Edge of Paved Shoulder, (E) Drainage Ditch boundaries, (E) ROW. 20200814. Base map sources: Caltrans Layout Map L-1, Project Number & Phase 01120003001, March 13, 2019; Caltrans Layout sheet L-1, file "0C550\_L-1.pdf (page 1 of 1)", produced by CT to DA on June 5, 2020 in response to PRAR for current L-1 sheet (site plan) with identified topographic contours.